



AGENDA
**HISTORIC PRESERVATION/
DESIGN REVIEW COMMISSION**

Members

- Chairperson Tim Siebert
- Vice Chair Peter Munck
- Alderperson Marc Christianson
- Barry Calnan
- Carol Molepske
- Janet Jurgella Finn (Alternate 1)
- Dan Kryst (Alternate 2)

Date and Time:	January 13, 2026 5:00 PM	Location:	Community Room 933 Michigan Avenue, Stevens Point, WI
			<u>OR</u>
			<u>Zoom Teleconferencing</u>
			Meeting ID: 831 6239 0141 Passcode: 466142
			<u>Computer:</u> https://us02web.zoom.us/j/83162390141?pwd=hNWMPG5b9NF2XPbbqM
			<u>Phone:</u> +1-312-626-6799 (US Chicago)

Opening Section:

1. Roll Call

Discussion and Possible Action on:

2. Report of the November 13, 2025 meeting of the Historic Preservation / Design Review Commission.
3. A request from Michael More, representing RA Cook & JI Altenburg Cooperative, for a demolition review to raze portions of the building located at 2041 Madison Street (Parcel ID 281240832403709), consistent with Ch. 22.05(2).
4. A request from Michael More, representing RA Cook & JI Altenburg Cooperative, for a design review to construct a building addition on the property located at 2041 Madison Street (Parcel ID 281240832403709), consistent with Ch. 22.05(2).
5. A request from the City of Stevens Point to raze the building located at 1039 Ellis Street (Parcel ID 281240832202053), consistent with Ch. 22.05(2).

Closing Section:

6. Adjourn

PLEASE TAKE NOTICE that any person who has special needs while attending these meetings or needs agenda materials for these meetings should contact the City Clerk as soon as possible to ensure that a reasonable accommodation can be made. The City Clerk can be reached by telephone at (715) 346-1569 or by mail at 1515 Strongs Avenue, Stevens Point, WI 54481.

Maps further defining the above area(s) may be obtained from the City of Stevens Point Department of Community Development, 1515 Strongs Avenue, Stevens Point, WI 54481, or by calling (715) 346-1567, during normal business hours.

PLEASE TAKE FURTHER NOTICE that a quorum of the Common Council may be in attendance at this meeting.

REPORT OF THE HISTORIC PRESERVATION/DESIGN REVIEW COMMISSION

November 13, 2025 – 5:00 PM
933 Michigan Ave, Stevens Point, WI 54481

PRESENT: Chair Siebert, Vice Chair Munck, Commissioner Jurgella Finn, Commissioner Calnan, and Commissioner Kryst

EXCUSED: Alderperson Christianson, Commissioner Molepske

ALSO PRESENT: Associate Planner / Zoning Administrator Kuhn

INDEX:

Opening Section:

1. Roll Call

Discussion and Possible Action on the Following:

2. Report of the October 23, 2025 meeting of the Historic Preservation / Design Review Commission.
3. A request from Nicole Terrill, representing Elodin Holdings LLC, for a design review to install a structure on the property located at 1220 Briggs Court (Parcel ID 281240832200137), consistent with Ch. 22.02(2).
4. A request from Robert Wilder to install a deck on the building located at 1724 Clark Street (Parcel ID 281240832103612), consistent with Ch. 22.05(2).
5. Overview of Zoning Code Rewrite:
 - Conservation Districts
 - City Historic Preservation Plan

Closing Section:

6. Adjourn

Opening Section:

1. Roll Call

Present: Siebert, Munck, Calnan, Jurgella Finn, and Kryst

Excused: Christianson, Molepske

Discussion and Possible Action on the Following:

2. Report of the October 23, 2025 meeting of the Historic Preservation / Design Review Commission.

Motion by Vice Chair Munck, to approve the October 23, 2025 minutes of the Historic Preservation/Design Review Commission; seconded by Commissioner Calnan.

Motion carried 5-0.

3. A request from Nicole Terrill, representing Elodin Holdings LLC, for a design review to install a structure on the property located at 1220 Briggs Court (Parcel ID 281240832200137), consistent with Ch. 22.02(2).

Nicole Terrell presented a request to install a second sauna structure on the property at 1220 Briggs Court. Associate Planner / Zoning Administrator Kuhn noted that a similar structure had been approved by the Commission years prior. The new unit matches the previously approved design and includes polycarbonate panels utilized to match the surrounding environment and provide passive solar heating.

Motion by Commissioner Calnan to approve the request to install a structure on the property located at 1220 Briggs Court; seconded by Commissioner Kryst.

Motion carried 5-0

4. A request from Robert Wilder to install a deck on the building located at 1724 Clark Street (Parcel ID 281240832103612), consistent with Ch. 22.05(2).

Austin Lamb, who recently purchased the property from Robert Wilder, presented his request for the demolition of an existing deck and the following installation of a new deck on the property located at 1724 Clark Street. The proposal meets existing design standards and zoning requirements.

Motion by Vice Chair Munck to approve the request to install a deck on the property located at 1724 Clark Street; seconded by Commissioner Kryst.

Motion carried 5-0

5. Overview of Zoning Code Rewrite:
 - Conservation Districts
 - City Historic Preservation Plan

Aldersperson Christianson attended virtually and was present for the Overview of the Zoning Code Rewrite.

Associate Planner / Zoning Administrator Kuhn provided an update on the City's Zoning Code Rewrite and requested Commission feedback on whether to include conservation districts for designated areas identified as historically eligible. Kuhn explained conservation districts regulate major changes and offer lighter restrictions than what a full historic district would require.

Commissioners discussed district conditions, potential benefits, and potential concerns of property owners within these proposed districts. Consensus was that no conservation districts should be pursued without conducting public feedback.

Closing Section:

6. Adjourn

Meeting adjourned at 5:40 PM.

A recording of this meeting can be viewed/heard at: <https://stevenspoint.com/365/AgendasMinutesVideos>

Administrative Staff Report

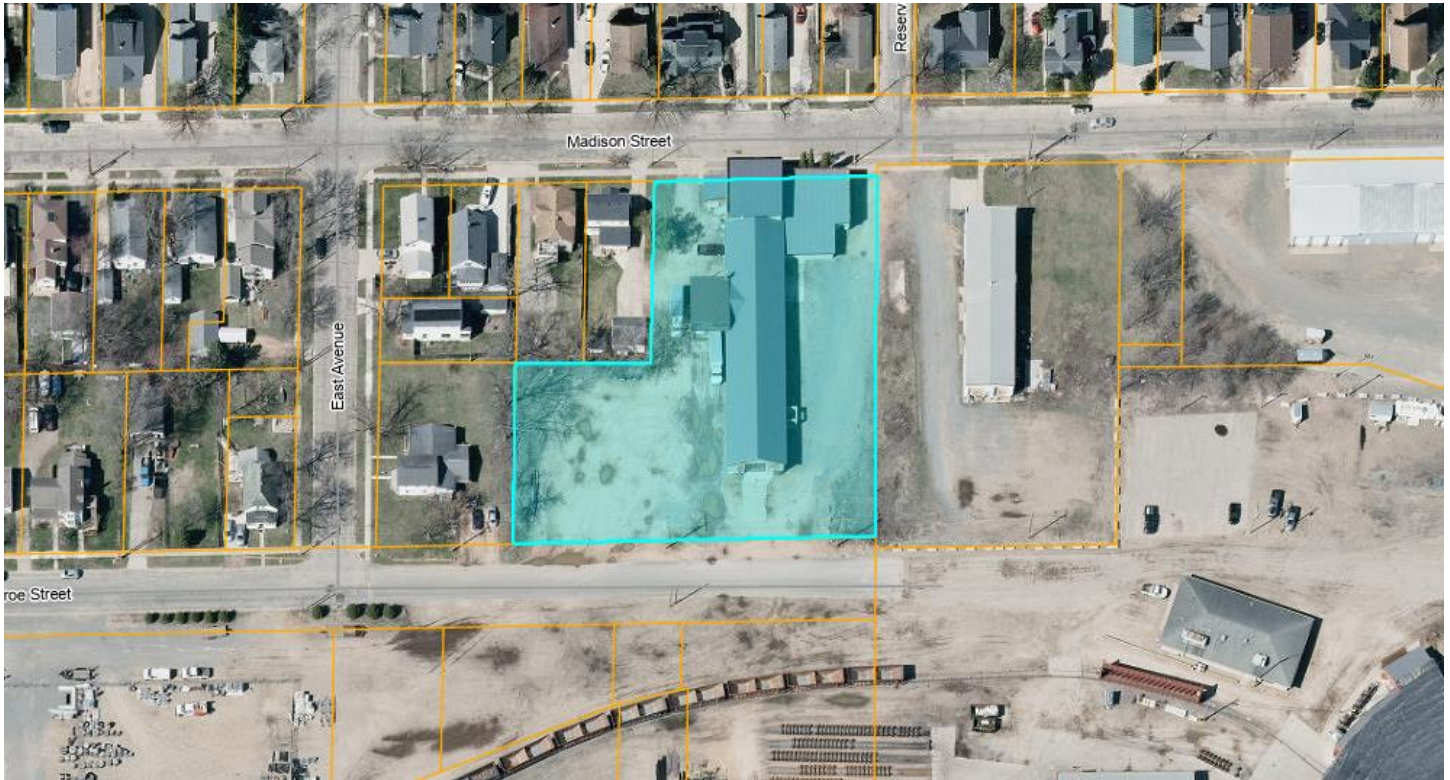


Demolition & Design Review 2041 Madison Street January 13, 2026

Department of Community Development
1515 Strongs Avenue, Stevens Point, WI
54481
Ph: (715) 346-1568 - Fax: (715) 346-1498

<p>Applicant(s):</p> <ul style="list-style-type: none"> Michael More, representing RA Cook & JI Altenburg Cooperative <p>Staff:</p> <ul style="list-style-type: none"> Adam Kuhn, Associate Planner akuhn@stevenspoint.com <p>Parcel Number(s):</p> <ul style="list-style-type: none"> 281240832403709 <p>Zone(s):</p> <ul style="list-style-type: none"> "M-2" Heavy Industrial <p>Council District:</p> <ul style="list-style-type: none"> District 5 – Ald. Birr <p>Lot Information:</p> <ul style="list-style-type: none"> Actual Frontage: 429 feet Effective Depth: 263 feet Square Footage: 56,628 Acreage: 1.30 <p>Planned Use:</p> <ul style="list-style-type: none"> Community Building <p>Applicable Regulations:</p> <ul style="list-style-type: none"> Chapter 22 Downtown Design Guidelines 	<p>Request</p> <p>A request from Michael More, representing RA Cook & JI Altenburg Cooperative, for a demolition review to raze portions of the building located at 2041 Madison Street (Parcel ID 281240832403709), consistent with Ch. 22.05(2).</p> <p>A request from Michael More, representing RA Cook & JI Altenburg Cooperative, for a design review to construct a building addition on the property located at 2041 Madison Street (Parcel ID 281240832403709), consistent with Ch. 22.05(2).</p> <p>City Official Design Review / Historic District</p> <ol style="list-style-type: none"> Local landmark property <p>Staff Recommendation</p> <p>Approve the request for a demolition review to raze portions of the building located at 2041 Madison Street (Parcel ID 281240832403709), subject to the following conditions:</p> <ol style="list-style-type: none"> A razing permit shall be obtained prior to the start of demolition. <p>Approve the request for a design review to construct a building addition on the property located at 2041 Madison Street (Parcel ID 281240832403709), subject to the following conditions:</p> <ol style="list-style-type: none"> A landscaping plan shall be submitted to the Designated Agent for review and approval. A building permit shall be obtained prior to the start of construction.
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Vicinity Map



Scope of Work

Overview: In 2024, the Common Council designated the subject property as a local landmark, at the request of the property owner. At the time, the intent for the local landmark designation request, in part, was to help facilitate an adaptive reuse project within the existing building. Being designated a local landmark property enables historic building code requirements to be used for interior work, thus adding some flexibility as part of the interior buildout. Enclosed in the agenda packet are interior plans for how the interior of the building is proposed to be used. The proposal includes two multi-activity spaces, a bakery, and a bar within the building. Please note that the bar/seating area identified in the plan set is subject to Plan Commission and Council approval, which is slated for consideration next month.

The demolition review requests to raze the loading dock situated on the west elevation of the building. The loading dock consists of cinder block siding material, which is inconsistent with the exterior façade material used for the remainder of the building (i.e., brick). Based on historic photographs, it is apparent that the loading dock was not original to the 1892 construction of the building.

The design review requests to construct two additions onto the building. Where the loading dock is proposed to be razed, the applicant seeks to construct a 73 sq ft vestibule addition that will serve as a required means of egress for uses within the building. Second, a 2,800 sq ft addition is proposed on the southeast corner of the building. This addition would serve the proposed bakery use, and would have a partial basement.

According to Section 7.4 of the Historic Preservation / Design Review Commission's Design Guidelines, building demolitions, in whole and in part, along with building additions, are considered a 'major work' and requires the review and approval of the Commission. Said requirement is outlined below.

Sec. 7.4: Major and Minor Works

Sec. 7.4.1 Major Works (HPDRC Approval)

- a. New construction or additions to primary buildings
- c. Demolition of any structure

The process to obtain a design review from the Commission is identified below.

CHAPTER 22: HISTORIC PRESERVATION

Division 5.02 Regulation of Construction, Reconstruction, Alteration, and Demolition

No owner or person in charge of a historic structure or historic site, or property located within a historic district shall reconstruct, alter, or demolish all or any part of the exterior of such property or construct any improvement upon such designated property or properties or cause or permit any such work to be performed upon such property or demolish such property unless approval has been granted by the commission.

Upon the filing of any request for a design review certificate with the commission, the commission shall review the request in accordance with the design guidelines. If the commission determines that the application for a design review certificate and the proposed changes are consistent with the design guidelines, it shall issue the design review certificate. Upon the issuance of such certificate, any other required permits shall be obtained.

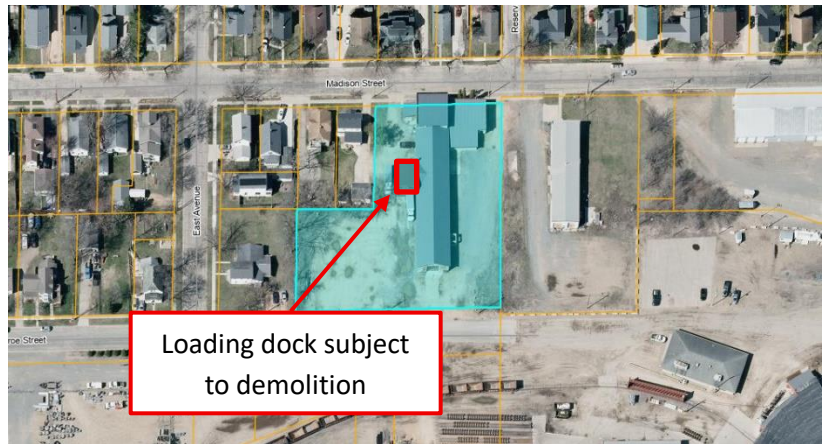
Guidelines of Review (numbers below pertain to the pertinent guideline standards)

***Other standards within the design guidelines not specifically mentioned below have been reviewed and are met or not applicable pertaining to the proposed building improvement activities.

SEC. 6.1.2: Demolition Guidelines

- 1. Whether the building or structure is of such architectural or historic significance that its demolition would be detrimental to the public interest and contrary to the general welfare of the people of the city or state.

Analysis: The building was constructed in 1892 and was designated as a local landmark in 2024. The loading dock was constructed after 1892, albeit the exact year of construction is not known.



Findings: The proposed demolition is not detrimental to the public interest and is not contrary to the general welfare. The loading dock is partially shielded from public view off of Madison Street. As the loading dock was built after the original building was constructed, and constructed in a way that is not consistent with the rest of the building, the proposed demolition will better achieve uniformity in architectural cohesiveness.

- 2. Whether the building or structure, although not itself a historic structure, contributes to the distinctive architectural or historic character of the historic district as a whole, and therefore, should be preserved

for the benefit of the people of the city or the state.

Analysis: The structure is a designated local landmark and is not part of a broader historic district.

Findings: The proposed demolition would not diminish the architectural and historic character of the site. The removal of the loading dock would eliminate the exterior appearance of the building as an industrial use, and would better appeal to land uses in surrounding buildings.

4. Whether the building or structure is of such old, unusual or uncommon design, texture, and/or material, that it could be reproduced only with great difficulty and/or expense.

Analysis: Cinder block is the exterior material used for the loading dock.

Findings: Cinder block is a common façade material, and in this instance is consistent with the building materials used on the remainder of the building.

7. Whether demolition of the building or structure would promote conformance with other criteria as designated in the City of Stevens Point Historic Design guidelines.

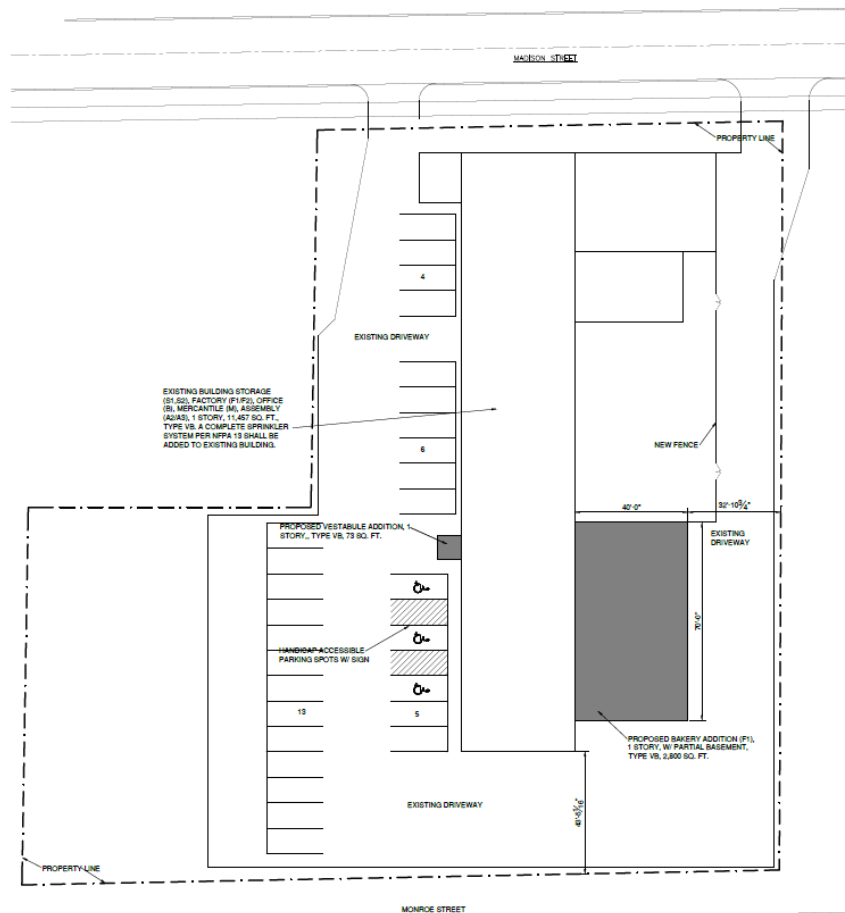
Analysis: Please see above. The proposed demolition would better meet the exterior wall guidelines as identified in the Commission’s Design Guidelines.

Findings: This standard is met.

SEC. 5.3: Addition Guidelines

- 1. Additions should be located to the rear or non-character defining elevation. With historic residential structures, additions should be placed in a manner that they are not clearly seen from the public right-of-way. Landscaping can often be used to minimize the visual impact that additions may have to the historic structure.**

Analysis: The proposed additions would be located along the side elevations of the buildings. While the addition would not be seen from Madison Street, it would be visible from Monroe Street. Monroe Street terminates into a turnaround at the east side lot line of the subject



Proposed Site Plan

property. As Monroe Street dead ends at the subject property, along with being adjacent to the Canadian National railyard, I believe that the landscape screening suggestion should be waived in this instance.

Findings: This standard is met.

2. New additions should not remove, damage, or obscure character-defining architectural features.

Analysis: The proposed vestibule addition would reintroduce more of the original exterior wall that is currently covered by the loading dock. Both additions would utilize the same roofing material as the rest of the building, while EIFS siding will be used. The lower portion of both additions will utilize a concrete veneer that detailed with a brick aesthetic to better match the texture and definition as the rest of the exterior walls on the building.

Findings: This standard is met.

3. Additions should be compatible in materials, design, roof form, and proportion to the main structure. However, new additions should be constructed at a scale smaller than the historic structure so as not to overpower the existing historic building.

Analysis: Please see item #2 above for details on the proposed siding and roof materials for the addition. Both additions are of small scale that would not overpower the remainder of the building that is original.

Findings: This standard is met.

4. Additions, like new construction, are representative of the time in which they are built. Therefore, contemporary designs are permitted, but should always be compatible with the existing historic structure.

Analysis: Both additions would utilize the same roofing material as the rest of the building, while EIFS siding will be used. The lower portion of both additions will utilize a concrete veneer that detailed with a brick aesthetic to better match the texture and definition as the rest of the exterior walls on the building.

Findings: The concrete base that is stamped to take shape comparable to brick would best match the siding pattern used on the remainder of the building, while trying not to mimic it by utilizing a different façade material.

5. An addition should never mimic or recreate the architecture of the primary historic structure.

Analysis: Please see item #4 above.

Findings: This standard is met.

6. Additions to historic structures should be clearly identifiable as such. Additions should be set back and constructed at a smaller scale than the original building. Architectural details should complement the main structure but should be clearly differentiated.

Analysis: As the subject property abuts streets along the north and south lot lines, the location of the proposed additions are intentionally centered in the middle of the lot to make it best be shown at a small scale from a public right-of-way. Both additions are relatively small in size, particularly in relation to the remainder of the building.

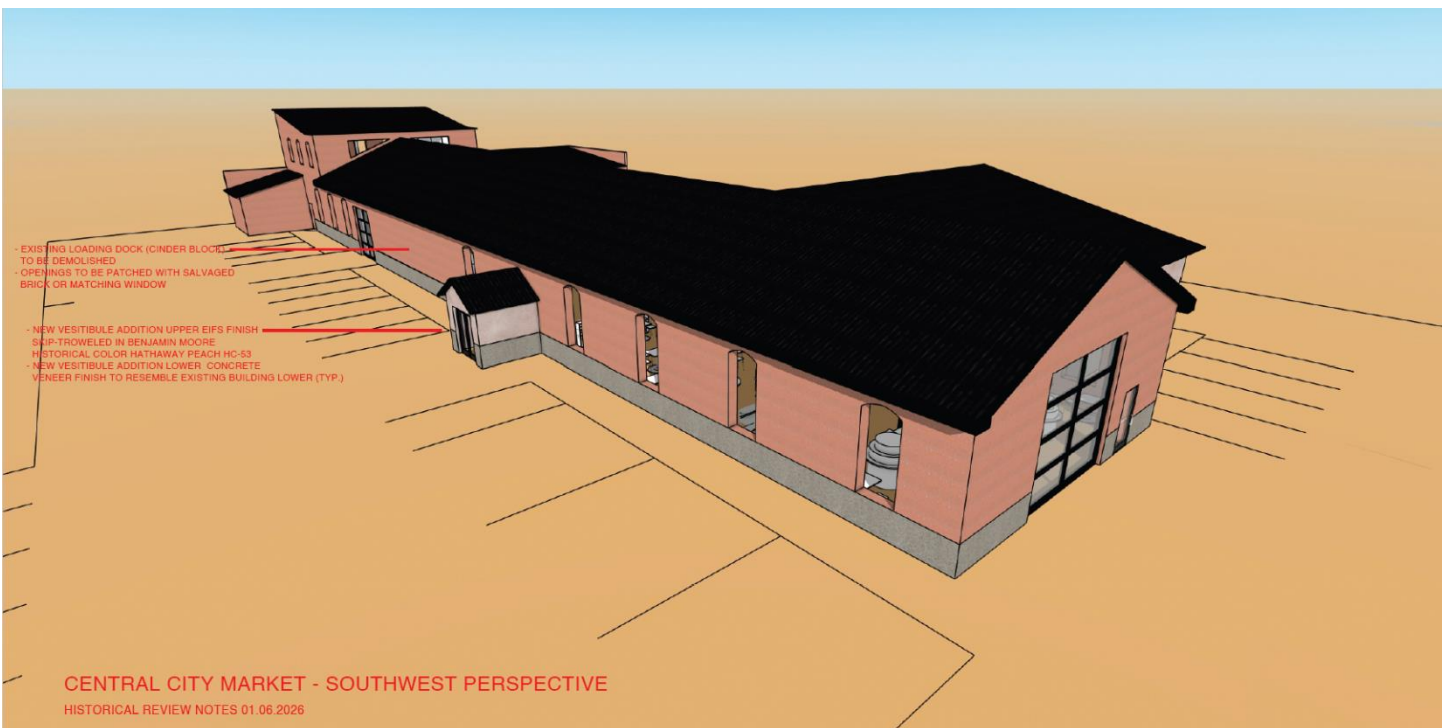
Findings: This standard is met.

Sec. 4.3.1: Landscaping Guidelines

1. Parking lots, driveways, and sidewalks shall comply with any ordinance requirements for size and landscaping elements as well as site grading.

Analysis: A parking lot is proposed that would be situated to the west of the existing building. The proposed parking lot meets minimum design standards identified in the City's Zoning Code (i.e., stall dimensions, aisle widths, pavement setbacks, ADA accessibility, etc.). A landscaping plan has not been submitted, but is required as part of the pending conditional use permit request.

Findings: As a condition of approval, a landscaping plan shall be submitted for review and approval. Said plan shall meet landscaping guidelines as identified in Section 4.3.1 of the Commission's Design Guidelines, as well as the Zoning Code.





HISTORIC PRESERVATION/ DESIGN REVIEW COMMISSION

City of Stevens Point
Community Development Department

1515 Strongs Avenue, Stevens Point, WI 54481
P: (715) 346-1567 F: (715) 346-1498
communitydevelopment@stevenspoint.com
<http://stevenspoint.com>

APPLICATION FOR DESIGN REVIEW

ADMINISTRATIVE SUMMARY (Staff Use Only)

Application #		Date Submitted		Assigned Case Manager	
Associated Permits or Applications (if any)				Pre-Application Conference Date	
Decision		Date Reviewed		Staff Signature	
Notes:					

APPLICANT/CONTACT INFORMATION

APPLICANT INFORMATION		CONTACT INFORMATION (Same as Applicant? <input type="checkbox"/>)	
Applicant Name	RA COOK & JI ALTENBURG COOPERATIVE	Contact Name	MICHAEL MORE
Address	2041 MADISON STREET	Address	1024 BUKOLT AVE
City, State, Zip	STEVENS POINT, WI 54481	City, State, Zip	STEVENS POINT, WI 54481
Telephone		Telephone	920-948-1322
Fax		Fax	
Email		Email	

OWNERSHIP INFORMATION

PROPERTY OWNER OF RECORD 1 INFORMATION (Same as Applicant? <input checked="" type="checkbox"/>)		PROPERTY OWNER OF RECORD 2 INFORMATION (If Needed)	
Owner's Name		Owner's Name	
Address		Address	
City, State, Zip		City, State, Zip	
Telephone		Telephone	
Fax		Fax	
Email		Email	

PROJECT SUMMARY

Subject Property Location [Please Include Address and Assessor's Identification Number(s)]		
Parcel 1	Parcel 2	Parcel 3
281-24-0832403709		
Legal Description of Subject Property		
LOT 1 CSM #10565- 47-145 BNG LOTS 1,2,3,10,11 12,13,14 BLK 6 HELMS ADD S32 T24 R8 1.30A 856110		
Area of Subject Property (Acres/Sq Ft)	Area of Building or Structure (Sq Ft)	
1.30	12,600	



Current Zoning District(s)		Current Historic District(s) - Local, State, National	
5		n/a	
Designated Future Land Use Category	Current Use of Property	Proposed Use of Property	
	MANUFACTURING	COMMERCIAL	
Briefly describe the proposed building, structure construction, reconstruction or exterior alteration. Please also provide rationale for the design review request, along with the time schedule (if any) for the project. (Use additional pages if necessary)			
<p>The proposed renovations include the following:</p> <ul style="list-style-type: none"> - Demolition of an existing loading dock (construction of cinder block, not original to the 1892 building) - Construction of a small vestibule on the west side as well as a kitchen on the east side. For both additions, the following apply: <ul style="list-style-type: none"> - roofing will match existing black standing seam - EIFS siding will be a hand-troweled finish with Benjamin Moore Hathaway Peach HC-53 - Lower portion concrete veneer shall match parged brick aesthetic of original building <p>Demolition to begin late winter / early spring, with construction of the additions estimated to begin mid 2026</p>			
Will the proposed work detrimentally change, destroy or adversely affect any exterior architectural features of the improvement upon which said work is to be done? Explain your answer.			
No. The demolition of the loading dock will restore the original appearance of the west side of the building. Any openings exposed by this demolition will be patched with similar materials or with windows consistent with the rest of the building. Both additions will be finished with color hues analogous to the original brick, with the lower portions sharing aesthetic consistency with the parged brick of the original building. The placement of doorways required for the additions will prioritize the use of existing windows to minimize the removal of the original brick walls.			
Does the proposed work match and harmonize with the external appearance of adjacent neighboring improvements. Explain your answer.			
Yes, see above answer.			
Does the proposed work conform to the objectives of the historic preservation plan for said district (if any)? Explain your answer.			
Yes. Utilizing historical colors and focusing on creating vibrancy for the community to experience this local historical landmark is consistent with the objectives of the preservation plan.			
Does the proposed work conform with the architectural design guidelines with emphasis on contextual issues including compatibility of size, volume proportions, rhythm, materials, detailing, colors, and expressiveness? (Historic Design Guidelines can be found at www.stevenspoint.com) Explain your answer.			
Yes. Utilizing an EIFS system for the additions allows us to emulate a masonry wall aesthetic, consistent with 3.2.2 of the Design Guidelines. Utilizing historical colors is consistent with 3.11 of the Design Guidelines. When possible, any venting required for modern infrastructure will be vented through the roof or additions, minimizing penetrations in the original brick walls.			

EXHIBITS

Letter to District Alderperson (www.stevenspoint.com/Directory)	<input type="checkbox"/>	Additional Exhibits If Any (List):
Photographs of Building or Structure	<input type="checkbox"/>	
Renderings or Elevations	<input checked="" type="checkbox"/>	
Site Plan (for additions, and new construction)	<input checked="" type="checkbox"/>	

CERTIFICATION AND SIGNATURE

By my signature below, I certify that the information contained in this application is true and correct to the best of my knowledge at the time of the application. I acknowledge that I understand and have complied with all of the submittal requirements and procedures and that this application is a complete application submittal. I further understand that an incomplete application submittal may cause my application to be deferred to the next posted deadline date.

Signature of Applicant	Date	Signature of Property Owner(s)	Date
			

January 8, 2026

Aldersperson Allison Birr
District #5

RE: REDEVELOPMENT AT THE RA COOK & JI ALTENBURG BUILDING, 2041 MADISON
STREET

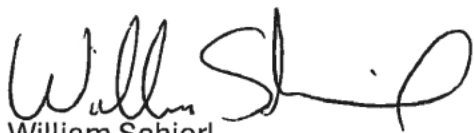
Dear Alder Birr,

As I mentioned in my email the Cooperative is now at the point of redevelopment and as applied for both a Conditional Use Permit for a pop-up bar as part of the Central City Market as well as a Historical Commission Review based on the building being on the Landmark registry. Please find them attached.

We are available at your convenience to walk through the building or blueprints to provide a full scope of the project.

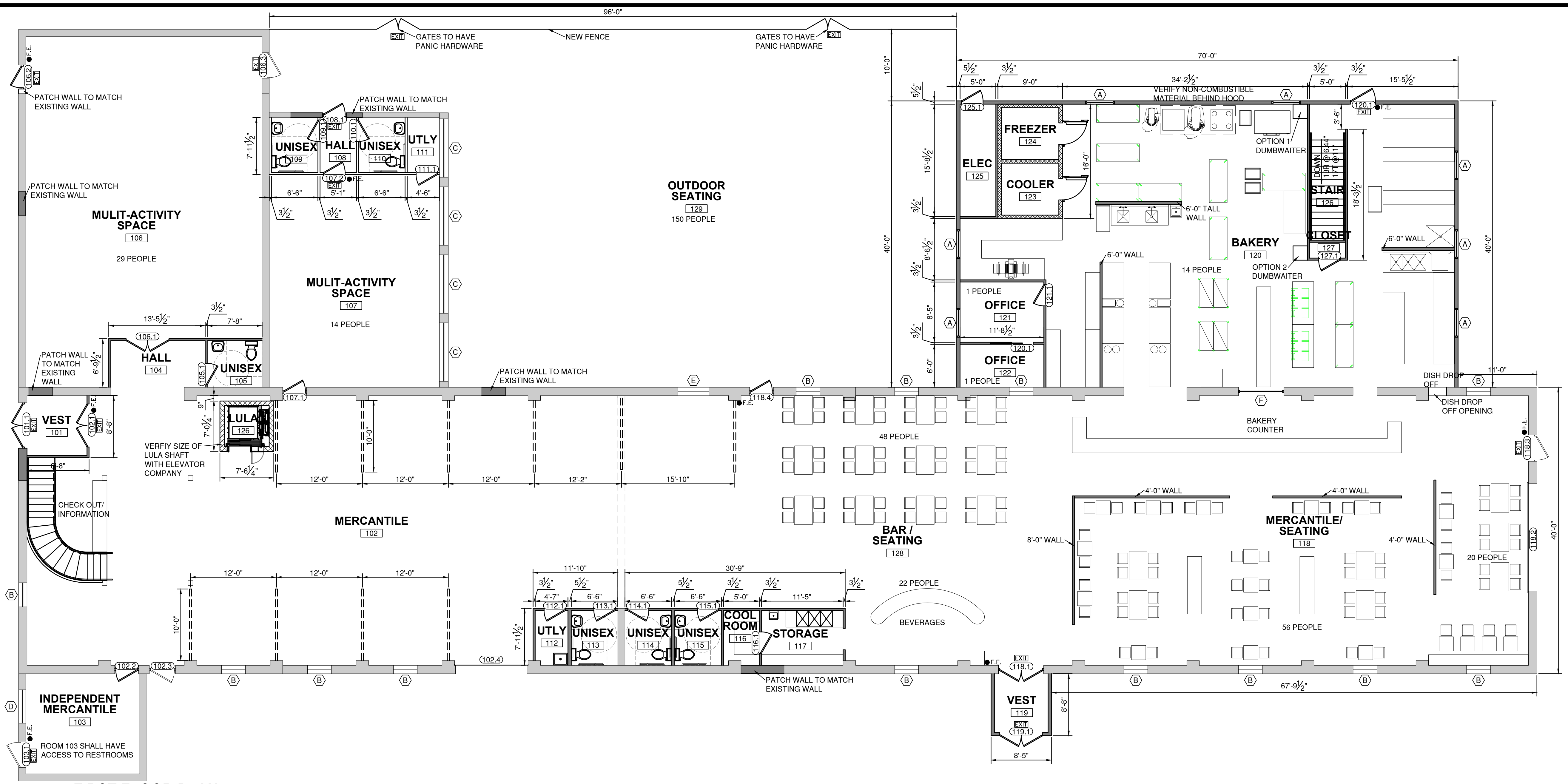
I can be reached at 715 345-5033 or

Sincerely,

A handwritten signature in black ink, appearing to read "William Schierl". The signature is fluid and cursive, with a large initial "W" and a long, sweeping tail.

William Schierl

Cook & Altenburg Cooperative
President



ROOM FINISH SCHEDULE

ROOM NUMBER	ROOM NAME	FLOOR	BASE	WALLS	CEILING	HEIGHT	NOTES
101	VEST	EXISTING SEALED CONCRETE	VINYL	PTD GYP BD	PTD GYP BD	9'-0"	
102	MERCANTILE	EXISTING SEALED CONCRETE	VINYL	EXISTING BRICK/NEW PTD GYP BD	EXISTING	VARIES	
103	INDEPENDENT MERCANTILE	EXISTING SEALED CONCRETE	VINYL	EXISTING	EXISTING	-	
104	HALL	EXISTING SEALED CONCRETE	VINYL	PTD GYP BD	SUSPENDED	9'-0"	
105	UNISEX	EXISTING SEALED CONCRETE	VINYL	EXISTING BRICK/NEW PTD GYP BD	EXISTING	9'-0"	
106	MULTI-ACTIVITY SPACE	WOOD FLOOR	VINYL	EXISTING BRICK/NEW PTD GYP BD	EXISTING	-	
107	MULTI-ACTIVITY SPACE	EXISTING SEALED CONCRETE	VINYL	EXISTING BRICK/NEW PTD GYP BD	PTD GYP BD	-	
108	HALL	EXISTING SEALED CONCRETE	VINYL	PTD GYP BD	PTD GYP BD	9'-0"	
109	UNISEX	EXISTING SEALED CONCRETE	VINYL	PTD MR SR	PTD MR SR	9'-0"	
110	UNISEX	EXISTING SEALED CONCRETE	VINYL	PTD MR SR	PTD MR SR	9'-0"	
111	UTLY	EXISTING SEALED CONCRETE	VINYL	PTD PLYWOOD	PTD PLYWOOD	9'-0"	
112	UTLY	EXISTING SEALED CONCRETE	VINYL	PTD PLYWOOD	PTD PLYWOOD	9'-0"	
113	UNISEX	EXISTING SEALED CONCRETE	VINYL	PTD MR SR	PTD MR SR	9'-0"	
114	UNISEX	EXISTING SEALED CONCRETE	VINYL	PTD MR SR	PTD MR SR	9'-0"	
115	UNISEX	EXISTING SEALED CONCRETE	VINYL	PTD MR SR	PTD MR SR	9'-0"	
116	COOL ROOM	SEALED CONCRETE	VINYL	PTD MR SR	SUSP VINYL SR	9'-0"	
117	STORAGE	SEALED CONCRETE	VINYL	PTD MR SR	SUSP VINYL SR	9'-0"	
118	MERCANTILE/SEATING	EXISTING SEALED CONCRETE	VINYL	EXISTING BRICK	EXISTING	VARIES	
119	VEST	SEALED CONCRETE	VINYL	PTD GYP BD	PTD GYP BD	9'-0"	
120	BAKERY	SEALED CONCRETE	VINYL	FRP OVER PLYWOOD	SUSP VINYL SR	9'-0"	
121	OFFICE	SEALED CONCRETE	VINYL	FRP OVER PLYWOOD	SUSP VINYL SR	9'-0"	
122	BAKERY	SEALED CONCRETE	VINYL	FRP OVER PLYWOOD	SUSP VINYL SR	9'-0"	
123	LDRY	SEALED CONCRETE	VINYL	PTD MR SR	SUSP VINYL SR	9'-0"	
124	UNISEX	SEALED CONCRETE	VINYL	PTD MR SR	SUSP VINYL SR	9'-0"	
125	ELEC	SEALED CONCRETE	VINYL	PTD PLYWOOD	PTD PLYWOOD	13'-4 1/2"	
126	LULA	SEALED CONCRETE	NONE	PTD PLYWOOD	PTD PLYWOOD	-	
127	CLOSET	SEALED CONCRETE	VINYL	PTD GYP BD	PTD GYP BD	9'-0"	

REMARKS

- ALL FINISHES SHALL COMPLY WITH IBC 2009 CHAPTER 8 & 12
- ALL SHEET ROCK TO BE 5/8" TAPED, LIGHT SAND TEXTURE, 2 COATS ENAMEL PAINT, SMOOTH AT TOILET ROOMS.
- M.R. S.R. - MOISTURE RESISTANT SHEET ROCK
- GRAB BARS TO BE "BRADLEY" MODEL #12 1 1/2" O.D. 18 GA. STAINLESS STEEL 18" 36" AND 42" LONG, CONCEALED MOUNTINGS
- F.E. - FIRE EXTINGUISHERS TO BE TYPE ABC, RECHARGEABLE, 10#. PROVIDE RECESSED CABINET.
- ALL MATERIALS TO BE AS SPECIFIED OR EQUAL
- EMERGENCY EXIT LIGHT W/ EGRESS LIGHTS & BATTERY PACK WITH EMERGENCY EXTERIOR LIGHT.
- TOILET ROOM BASE SHALL BE A MINIMUM OF 4" IN HEIGHT
- PARTITION WALLS TO BE INSULATED WITH FIBERGLASS SOUND BATTS.
- PROVIDE ADA TOILET ROOM SIGNS AND VERTICAL HANDICAP PARKING SIGNS.

DOOR SCHEDULE

DOOR NUMBER	DOOR SIZE	DESCRIPTION	HARDWARE	NOTES
101.1	(2) 3'-0"x7'-0"	NEW DOUBLE ALUMINUM STOREFRONT DOOR W/ CLOSER	PANIC	
102.1	(2) 3'-0"x7'-0"	NEW DOUBLE ALUMINUM STOREFRONT DOOR W/ CLOSER	PANIC	
102.2	(2) 3'-0"x7'-0"	NEW INSULATED STEEL DOOR W/ CLOSER	STANDARD EXIT	
102.3	3'-4"x7'-0"	NEW DOUBLE ALUMINUM STOREFRONT DOOR W/ CLOSER	STANDARD EXIT	
102.4	10'-0"x11'-0"	EXISTING INSULATED OVERHEAD DOOR	OPERATOR	
102.5	3'-4"x7'-0"	EXISTING INSULATED STEEL DOOR W/ CLOSER	STANDARD EXIT	
103.1	3'-0"x7'-0"	EXISTING STEEL INSULATED DOOR W/ HALF GLASS W/ CLOSER	PANIC	
105.1	3'-0"x7'-0"	NEW WOOD DOOR	PRIVACY	
106.1	(2) 3'-0"x7'-0"	NEW DOUBLE DOOR	STANDARD EXIT	
106.2	3'-0"x7'-0"	NEW ALUMINUM STOREFRONT DOOR W/ CLOSER	PANIC	
106.3	3'-4"x7'-0"	EXISTING INSULATED STEEL DOOR W/ CLOSER	PANIC	
107.1	(2) 3'-0"x7'-0"	NEW DOUBLE DOOR	STANDARD EXIT	
107.2	3'-0"x7'-0"	NEW WOOD DOOR	PANIC	
108.1	3'-0"x7'-0"	NEW INSULATED STEEL DOOR W/ CLOSER	PANIC	
109.1	3'-0"x7'-0"	NEW WOOD DOOR	PRIVACY	
110.1	3'-0"x7'-0"	NEW WOOD DOOR	PRIVACY	
111.1	3'-0"x7'-0"	NEW WOOD DOOR	STANDARD EXIT	
112.1	3'-0"x7'-0"	NEW WOOD DOOR	STANDARD EXIT	
113.1	3'-0"x7'-0"	NEW WOOD DOOR	PRIVACY	
114.1	3'-0"x7'-0"	NEW WOOD DOOR	PRIVACY	
115.1	3'-0"x7'-0"	NEW WOOD DOOR	PRIVACY	
116.1	3'-0"x7'-0"	NEW INSULATED STEEL DOOR W/ CLOSER	STANDARD EXIT	
118.1	(2) 3'-0"x7'-0"	NEW DOUBLE ALUMINUM STOREFRONT DOOR W/ CLOSER	PANIC	
118.2	15'-0"x14'-0"	EXISTING INSULATED GLASS OVERHEAD DOOR	OPERATOR	
118.3	3'-4"x7'-0"	EXISTING INSULATED STEEL DOOR W/ CLOSER	PANIC	
118.4	3'-0"x7'-0"	NEW DOUBLE ALUMINUM STOREFRONT DOOR W/ CLOSER	PANIC	
119.1	(2) 3'-0"x7'-0"	NEW DOUBLE ALUMINUM STOREFRONT DOOR W/ CLOSER	PANIC	
120.1	3'-6"x7'-0"	EXISTING INSULATED STEEL DOOR W/ CLOSER	STANDARD EXIT	
121.1	3'-0"x7'-0"	NEW WOOD DOOR	STANDARD EXIT	
122.1	3'-0"x7'-0"	NEW WOOD DOOR	STANDARD EXIT	
125.1	3'-0"x7'-0"	NEW INSULATED STEEL DOOR W/ CLOSER	PANIC	
127.1	3'-0"x7'-0"	NEW WOOD DOOR	PRIVACY	

WINDOW SCHEDULE

WINDOW TAG	WINDOW SIZE	DESCRIPTION	NOTES
A	4'-0"x1'-6"	NEW FIXED VINYL WINDOW W/ SILL MOUNTED AT 7'-0"	
B	3'-4"x8'-0"	EXISTING WINDOW	
C	8'-2"x1'-4"	EXISTING WINDOW	
D	4'-0"x4'-8"	EXISTING WINDOW	
E	3'-4"x5'-4"	EXISTING WINDOW	
F	6'-4"x8'-0"	NEW STOREFRONT WINDOW	VERIFY SIZE

-WINDOW LOCATIONS ARE IN EXISTING BRICKED IN WINDOWS.
-WINDOWS TO BE INSULATED LOW 'E' GLASS

DRAWN BY: SLS

CHECKED BY: ---

PLAN SET:
PRELIMINARY

DATE:
10/27/2025

SCALE:
AS NOTED

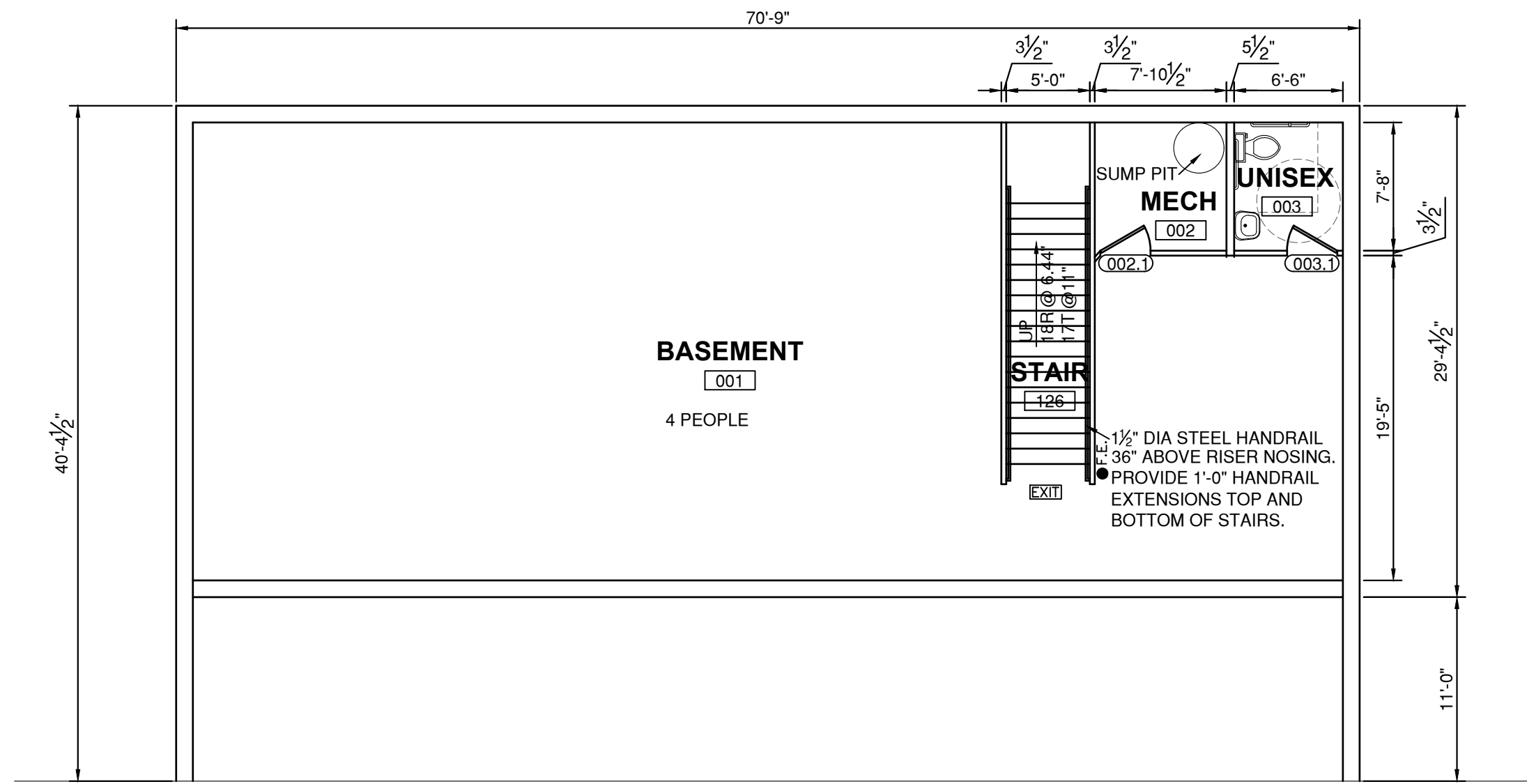
REVISION:

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SHEET DISCUSSION:
FIRST FLOOR PLAN

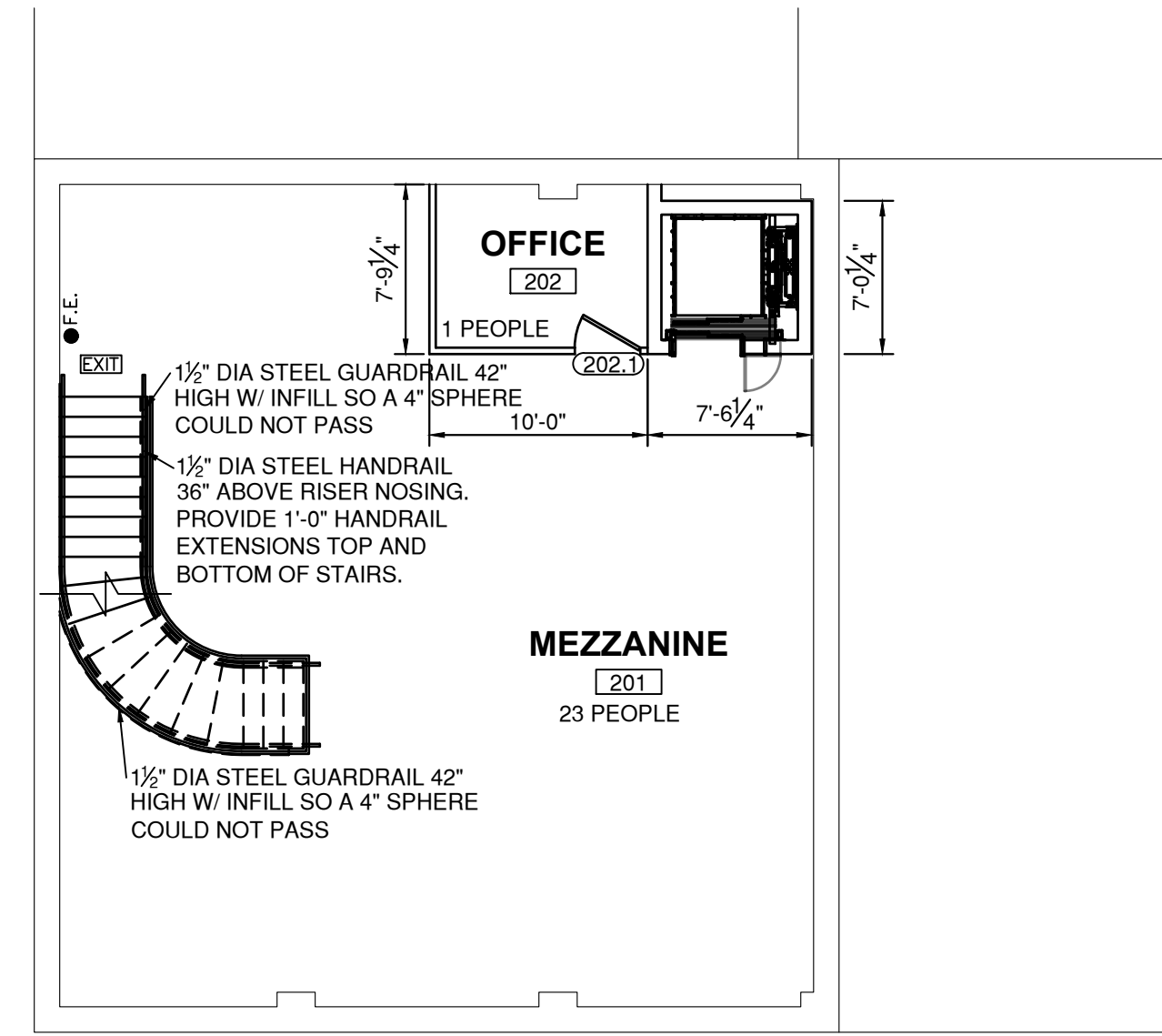
SHEET SIZE: ARCH D 36X24

SHEET:



1 BASEMENT FLOOR PLAN

SCALE: 1/8"=1'-0"



2 MEZZANINE FLOOR PLAN

SCALE: 1/8"=1'-0"



ROOM FINISH SCHEDULE

ROOM NUMBER	ROOM NAME	FLOOR	BASE	WALLS	CEILING	HEIGHT	NOTES
001	BASEMENT	EXISTING SEALED CONCRETE	NONE	CONCRETE	PRECAST	9'-0"	
002	MECH	EXISTING SEALED CONCRETE	NONE	CONCRETE	PRECAST	-	
003	UNISEX	EXISTING SEALED CONCRETE	NONE	CONCRETE	PRECAST	9'-0"	
201	MEZZANINE	EXISTING	VINYL	EXISTING	EXISTING	9'-0"	
202	OFFICE	EXISTING	VINYL	PTD GYP BD	SUSPENDED		

- REMARKS
- ALL FINISHES SHALL COMPLY WITH IBC 2009 CHAPTER 8 & 12
 - ALL SHEET ROCK TO BE 5/8" TAPED, LIGHT SAND TEXTURE, 2 COATS ENAMEL PAINT, SMOOTH AT TOILET ROOMS.
 - M.R. S.R. = MOISTURE RESISTANT SHEET ROCK
 - GRAB BARS TO BE "BRADLEY" MODEL 812 1 1/2" O.D. 18 GA. STAINLESS STEEL 18", 36" AND 42" LONG, CONCEALED MOUNTINGS
 - F.E. = FIRE EXTINGUISHERS TO BE TYPE ABC, RECHARGEABLE, 10#. PROVIDE RECESSED CABINET.
 - ALL MATERIALS TO BE AS SPECIFIED OR EQUAL
 - EMERGENCY EXIT LIGHT W/ EGRESS LIGHTS & BATTERY PACK WITH EMERGENCY EXTERIOR LIGHT.
 - TOILET ROOM BASE SHALL BE A MINIMUM OF 4" IN HEIGHT
 - PARTITION WALLS TO BE INSULATED WITH FIBERGLASS SOUND BATTS.
 - PROVIDE ADA TOILET ROOM SIGNS AND VERTICAL HANDICAP PARKING SIGNS.

DOOR SCHEDULE

DOOR NUMBER	DOOR SIZE	DESCRIPTION	HARDWARE	NOTES
002.1	3'-0"X7'-0"	NEW WOOD DOOR	STANDARD EXIT	
003.1	3'-0"X7'-0"	NEW WOOD DOOR	PRIVACY	
202.1	3'-0"X7'-0"	NEW WOOD DOOR	STANDARD EXIT	

- ALL LOCKSETS TO BE LEVER TYPE AND ADA COMPLIANT.
- ALL EXTERIOR DOORS SHALL HAVE ADA THRESHOLD, WEATHER STRIPPING, & CLOSER. LATCH GUARD ON DOOR 8.
- ALL FIRE DOORS SHALL HAVE LABELED DOORS AND FRAMES.

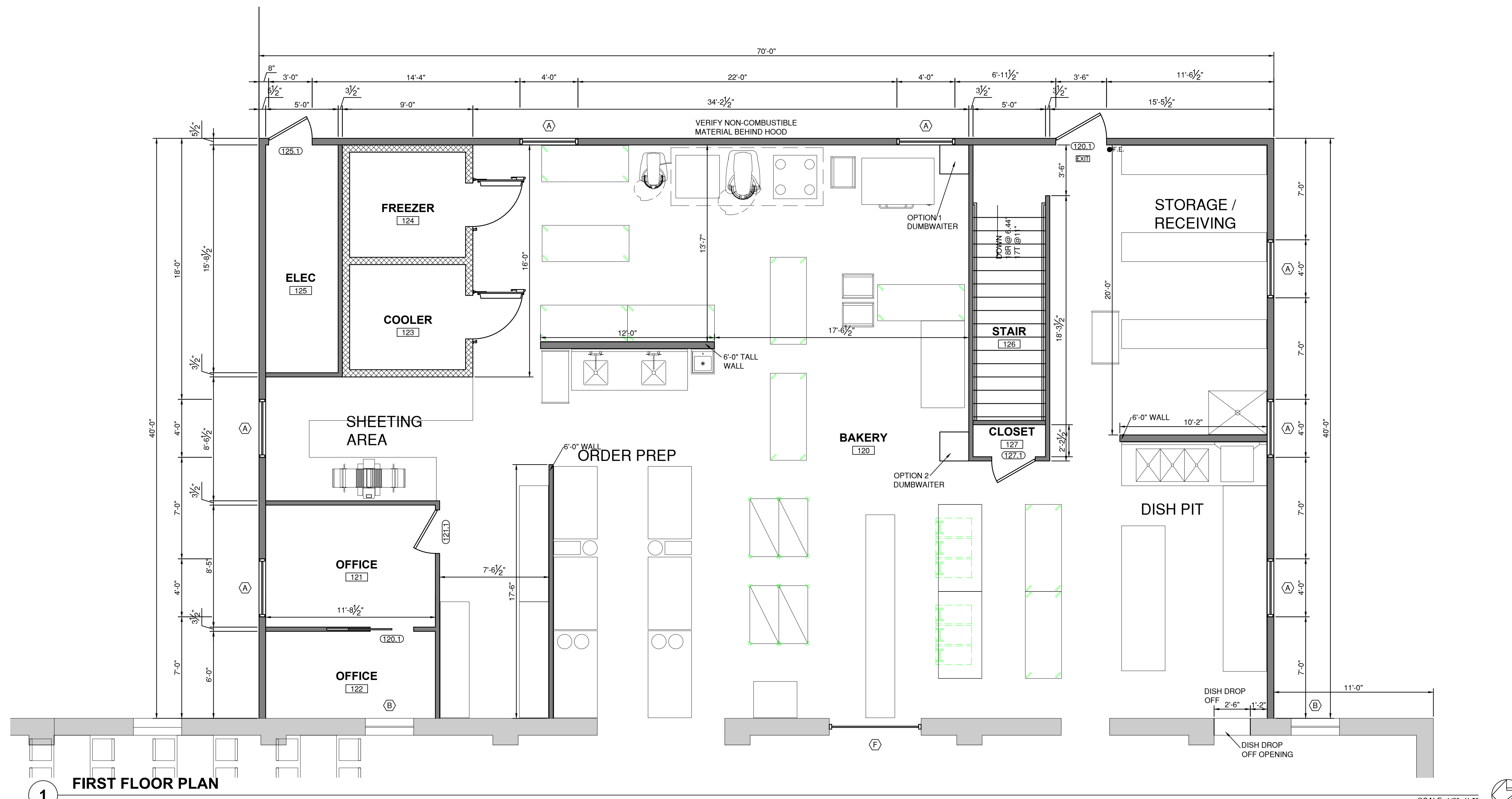
RISC AMER. ALTHOUGH DILIGENT EFFORTS HAVE BEEN MADE IN THE PREPARATION AND CHECKING OF THESE PLANS FOR ACCURACY, ALL CONTRACTORS SHALL CHECK, VERIFY AND BE RESPONSIBLE FOR ALL DETAILS AND DIMENSIONS. ALL CONTRACTORS SHALL EXAMINE THE SITE AND EXISTING CONDITIONS OF THE PROPOSED WORK. ALL CONTRACTORS SHALL RETURN ALL PLAN PAGES FOR WORK IN THEIR CONTRACT. ALL WORK ON THIS PROJECT IS TO BE GOVERNED BY THE LATEST EDITION OF THE CODES. ENGINEER, ARCHITECT AND CONTRACTOR SHALL BE RESPONSIBLE FOR EXISTING OR PRE-EXISTING SOIL OR STRUCTURAL CONDITIONS. THE ENGINEER WILL NOT BE RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES, OR PROCEDURES, OR FOR SAFETY PRECAUTIONS AND PROGRAMS IN CONNECTION WITH THE WORK, AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONTRACT DOCUMENTS. CONTRACTOR SHALL REVIEW ALL SHOP DRAWINGS PRIOR TO SUBMITTAL TO THE ENGINEER.

RATSCH ENGINEERING COMPANY, LTD.
 547 HEWETT ST.
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 FAX: 715.743.4469
 WWW.RATSCHENGINEERING.COM

ALTENBURG - CENTER CITY MARKET
 PROPOSED MERCANTILE/ASSEMBLY ADDITIONAL/ALTERATION
 2041 MADISON STREET STEVENS POINT, WISCONSIN 54481

PROJECT NAME:
 PROJECT DESCRIPTION:
 PROJECT ADDRESS:
 DRAWN BY: S.S.
 CHECKED BY: ---
 PLAN SET:
PRELIMINARY
 DATE:
10/27/2025
 SCALE:
AS NOTED
 REVISION:

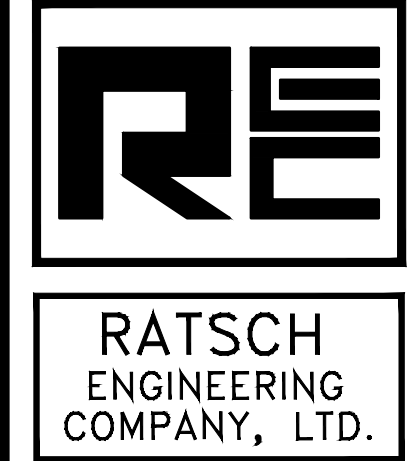
 SHEET DISCUSSION:
MEZZANINE & BASEMENT FLOOR PLANS
 SHEET SIZE: ARCH D 36X24
SHEET:
A1.2



1 FIRST FLOOR PLAN

SCALE: 1/8"=1'-0"

DISCLAIMER: ALL INFORMATION CONTAINED HEREIN IS FOR INFORMATIONAL PURPOSES ONLY. THE ENGINEER HAS CONDUCTED VISUAL GENERAL VERIFICATION OF THE INFORMATION PROVIDED AND HAS NOT CONDUCTED A FIELD SURVEY. THE ENGINEER HAS NOT CONDUCTED A FIELD SURVEY OF THE SITE AND DOES NOT WARRANT THE ACCURACY OF THE INFORMATION PROVIDED. THE ENGINEER HAS NOT CONDUCTED A FIELD SURVEY OF THE SITE AND DOES NOT WARRANT THE ACCURACY OF THE INFORMATION PROVIDED. THE ENGINEER HAS NOT CONDUCTED A FIELD SURVEY OF THE SITE AND DOES NOT WARRANT THE ACCURACY OF THE INFORMATION PROVIDED.



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ALTENBURG - CENTER CITY MARKET
 PROPOSED MERCANTILE/ASSEMBLY ADDITIONAL/ALTERATION
 2041 MADISON STREET STEVENS POINT, WISCONSIN 54481

PROJECT NAME:	ALTENBURG - CENTER CITY MARKET
PROJECT DESCRIPTION:	PROPOSED MERCANTILE/ASSEMBLY ADDITIONAL/ALTERATION
PROJECT ADDRESS:	2041 MADISON STREET STEVENS POINT, WISCONSIN 54481
DRAWN BY:	SL
CHECKED BY:	
PLAN SET:	PRELIMINARY
DATE:	10/27/2025
SCALE:	AS NOTED
REVISION:	
SHEET DESCRIPTION:	ENLARGED BAKERY FLOOR PLAN
SHEET SIZE:	ARCH D 36X24
SHEET:	A1.3



**RATSCH
ENGINEERING
COMPANY, LTD.**

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ALTENBURG - CENTER CITY MARKET

PROPOSED MERCANTILE/ASSEMBLY ADDITIONAL/ALTERATION
2041 MADISON STREET STEVENS POINT, WISCONSIN 54481

PROJECT NAME:
PROJECT DESCRIPTION:
PROJECT ADDRESS:

DRAWN BY: *SL*
CHECKED BY: ---

PLAN SET:
PRELIMINARY

DATE:
10/27/2025

SCALE:
AS NOTED

REVISION:

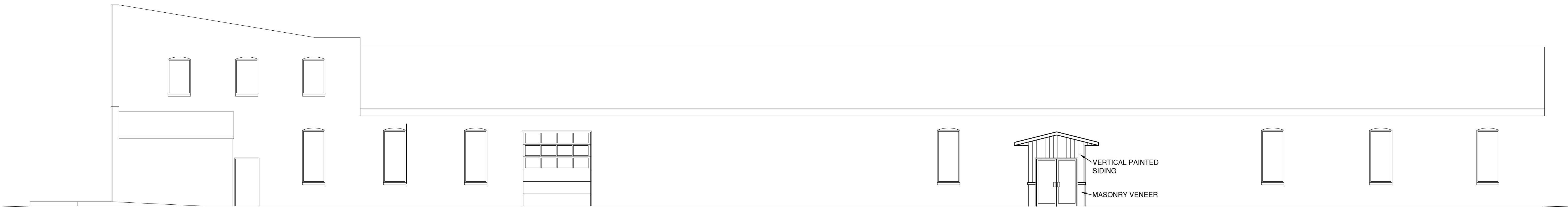
NO.	DESCRIPTION

SHEET SIZE: ARCH D 36X24

SHEET DESCRIPTION:
ELEVATIONS

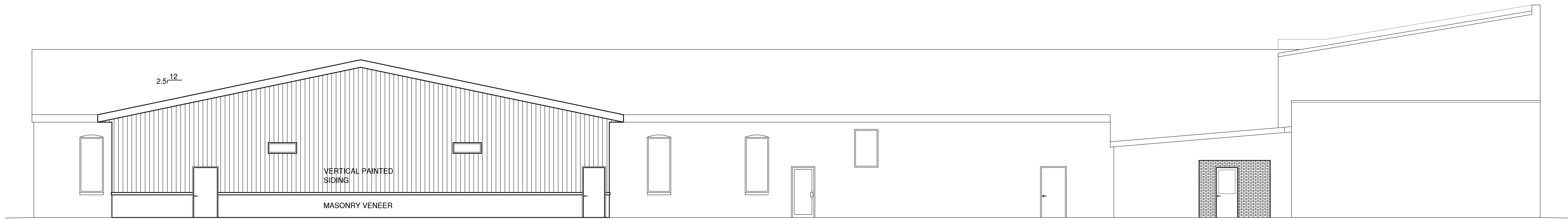
SHEET:
A2.1

DISCLAIMER: THESE PLANS OR ANY PART THEREOF HAVE BEEN PREPARED AND CHECKED BY THE ARCHITECT OR ENGINEER AND ARE NOT TO BE USED FOR ANY OTHER PROJECT OR PURPOSE WITHOUT THE WRITTEN CONSENT OF RATSCH ENGINEERING COMPANY, LTD. THE ARCHITECT OR ENGINEER WILL NOT BE RESPONSIBLE FOR ANY CONSTRUCTION METHODS, MATERIALS, OR PROCEDURES NOT SPECIFIED IN THESE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR THE SAFETY OF THE WORK. THE ARCHITECT OR ENGINEER SHALL NOT BE RESPONSIBLE FOR ANY DAMAGE TO PERSONS OR PROPERTY CAUSED BY THE WORK OR FOR ANY OTHER REASON. ALL SHOP DRAWINGS SHALL BE REVIEWED BY THE ARCHITECT OR ENGINEER PRIOR TO SUBMITTAL TO THE CONTRACTOR.



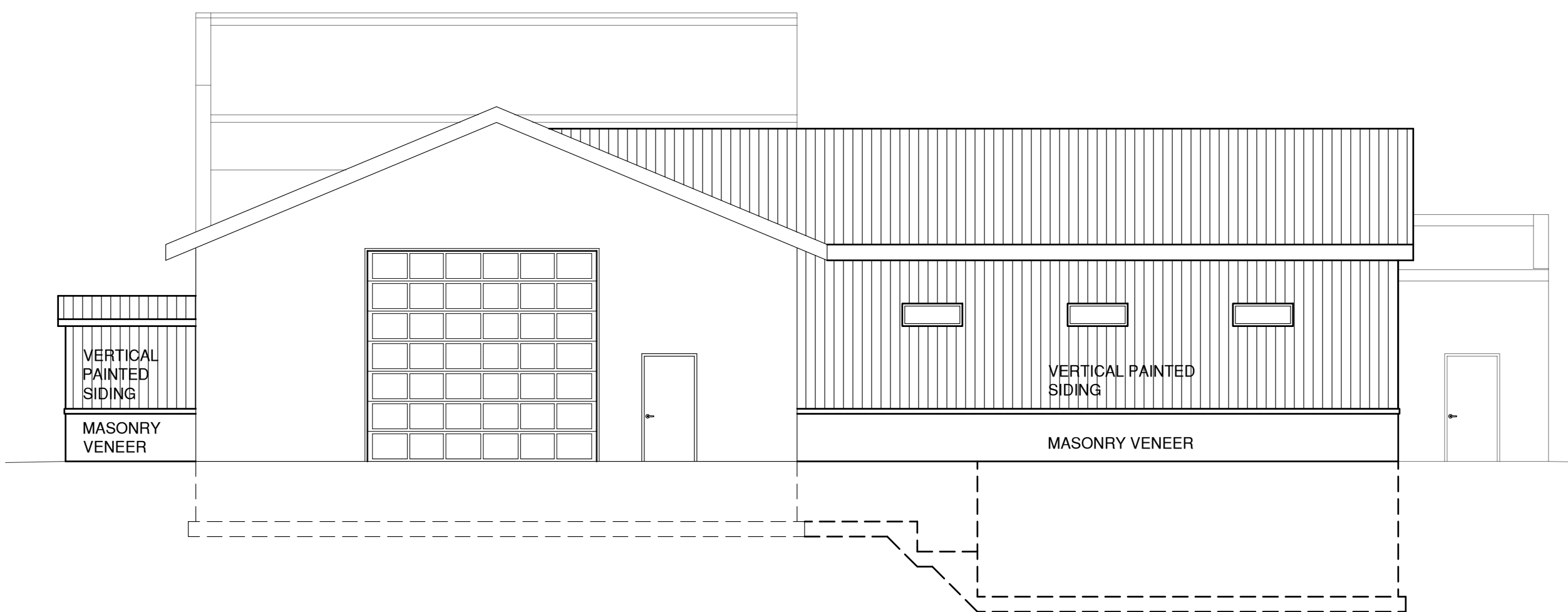
1 WEST ELEVATION

SCALE: 1/8"=1'-0"



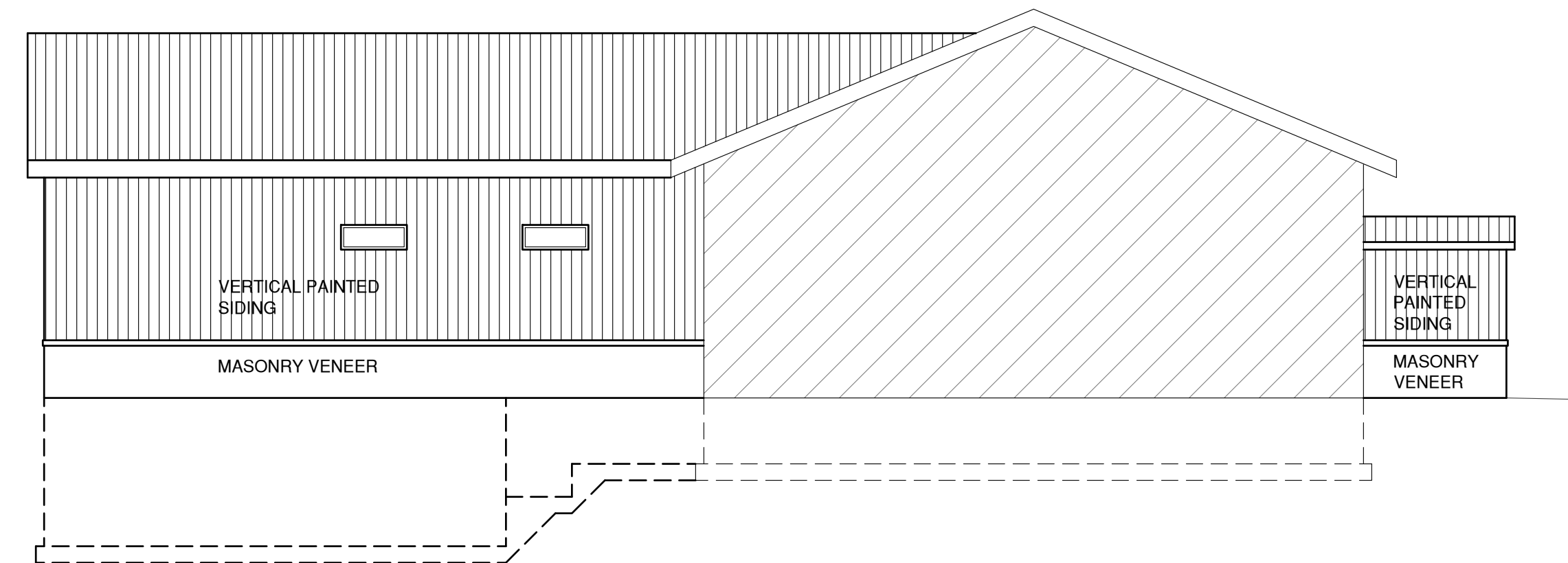
2 EAST ELEVATION

SCALE: 1/8"=1'-0"



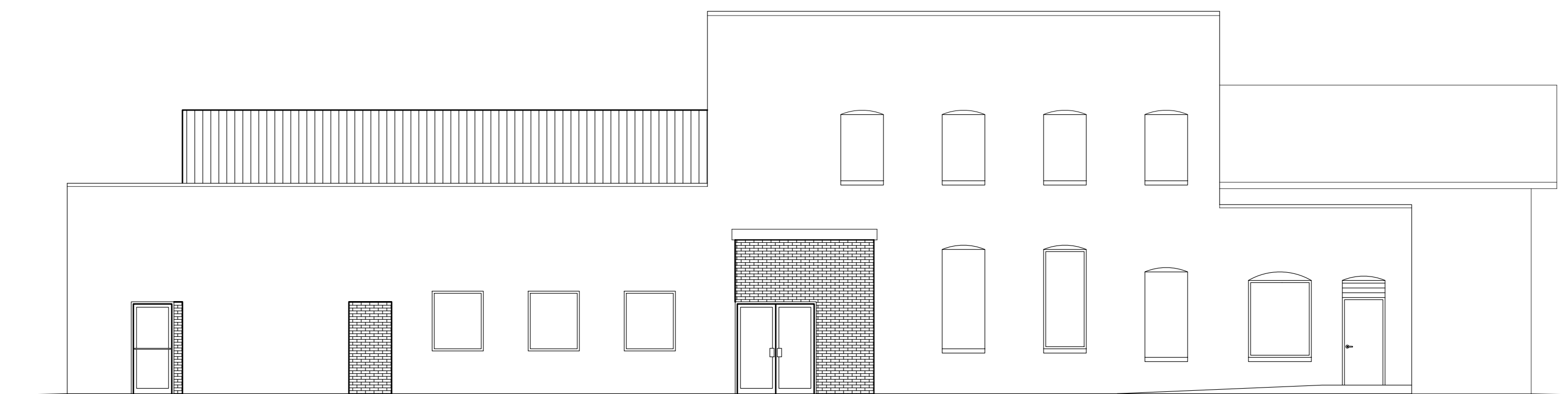
3 SOUTH ELEVATION

SCALE: 1/8"=1'-0"



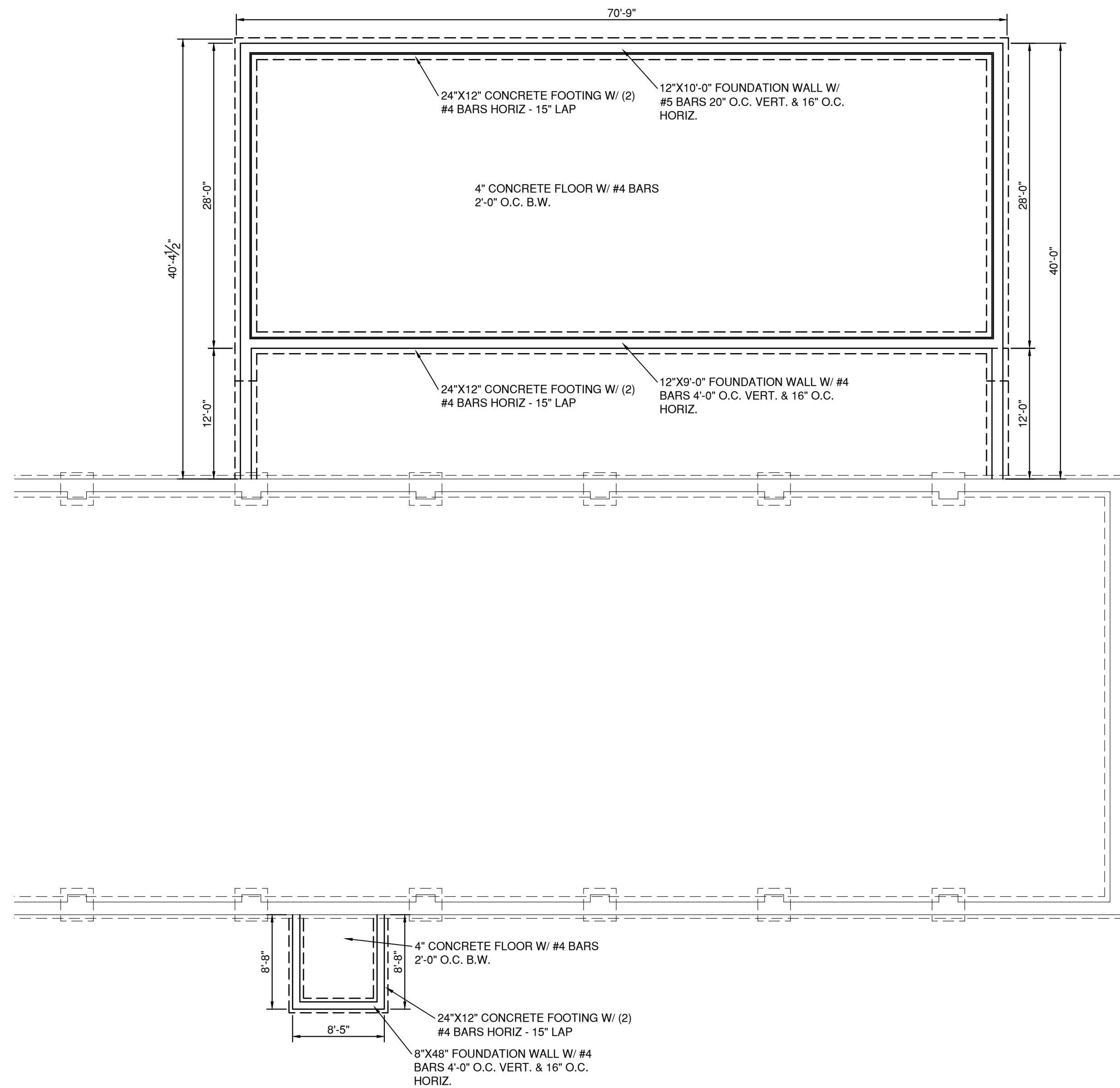
4 NORTH ELEVATION - CUT-THROUGH

SCALE: 1/8"=1'-0"

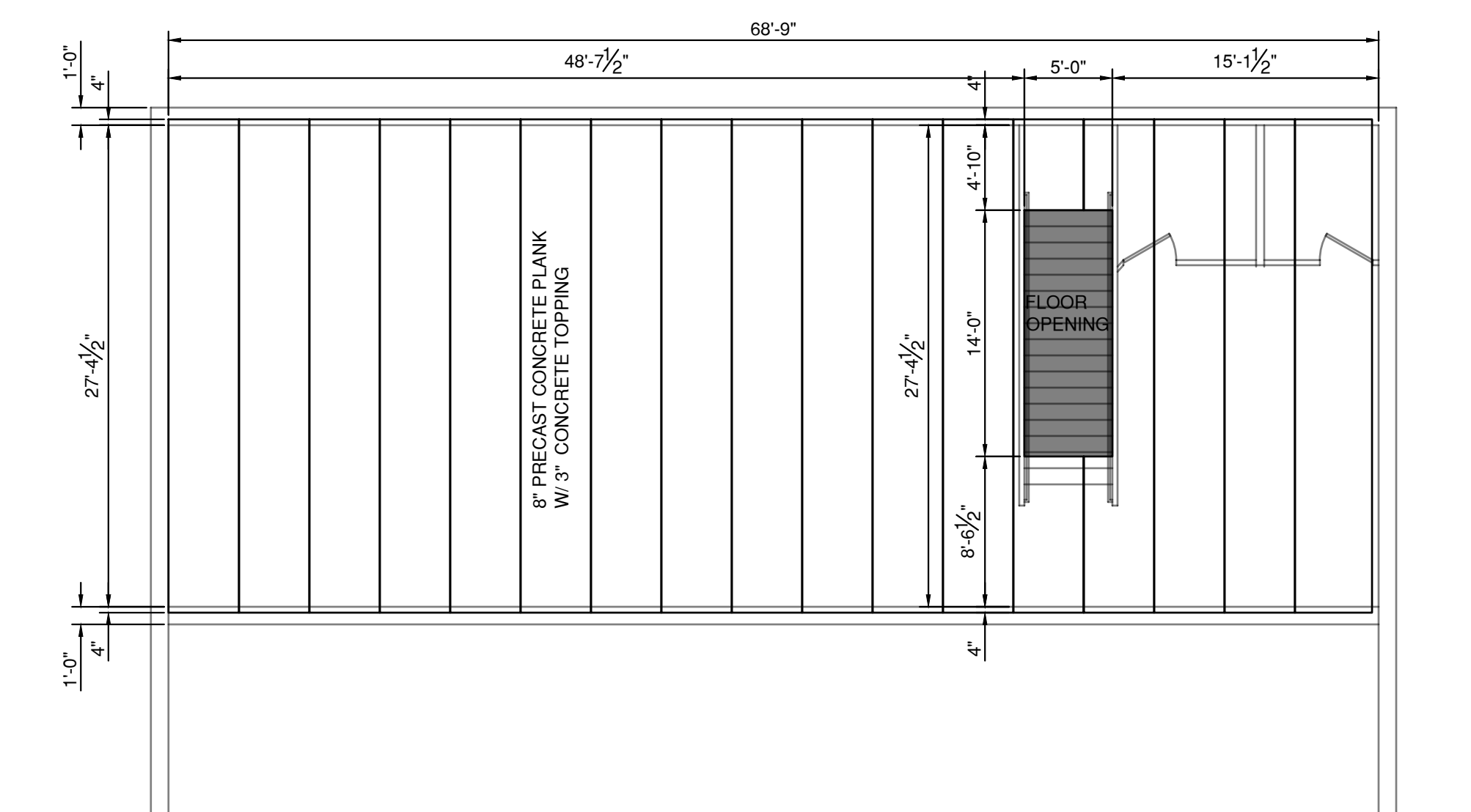


5 NORTH ELEVATION

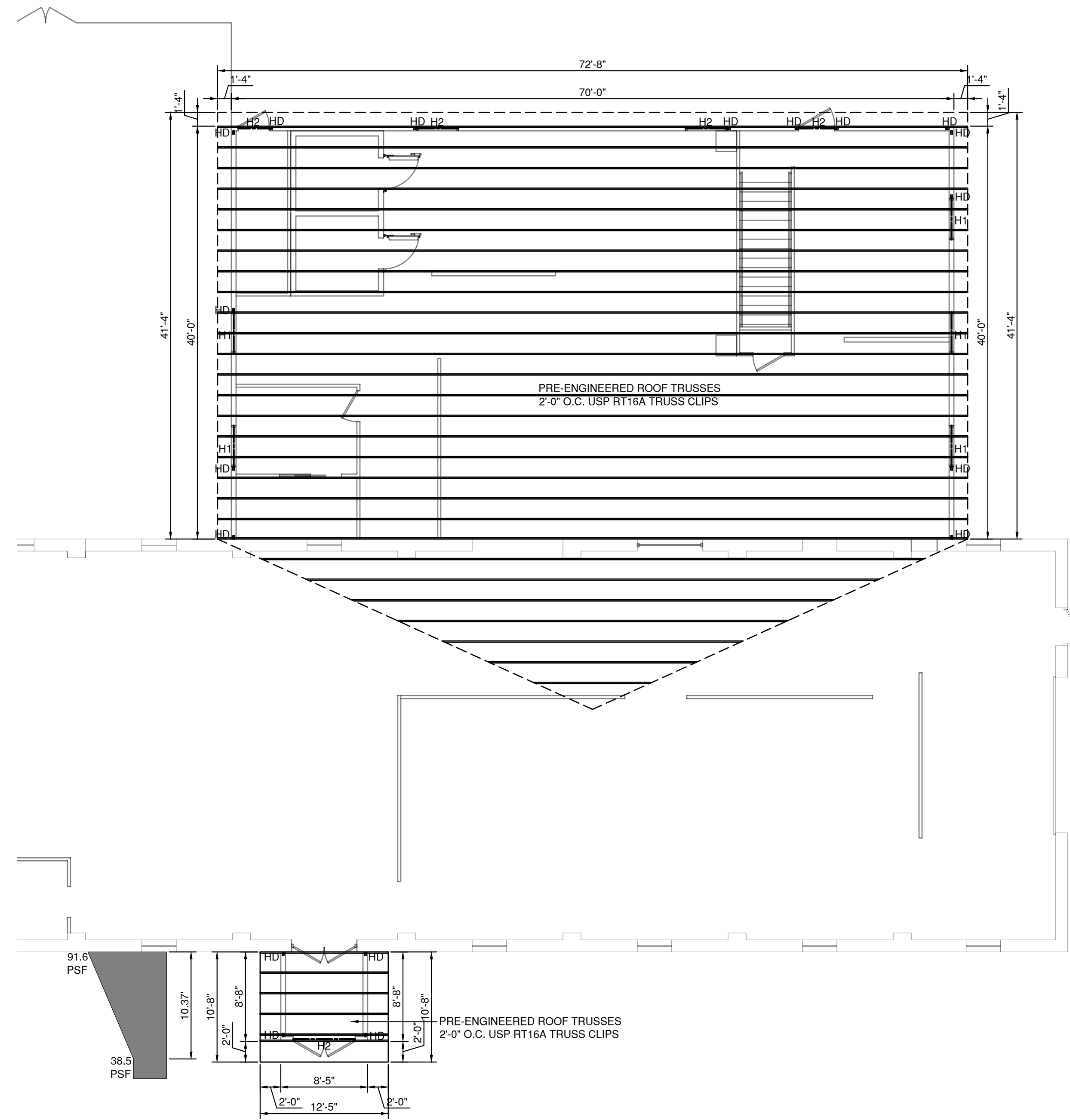
SCALE: 1/8"=1'-0"



1 FOUNDATION PLAN SCALE: 1/8"=1'-0"



3 FIRST FLOOR FRAMING PLAN SCALE: 1/8"=1'-0"



2 ROOF FRAMING PLAN SCALE: 1/8"=1'-0"

HEADERS AND HOLDDOWNS:
 H1 - (2) 1 3/4"X 1/2" MICROLAM LVL W/ (3) SHOULDER STUDS EACH END
 H2 - (2) 2X5 SPF #2 W/ (1) SHOULDER STUD EACH END
 HD - HOLDDOWN ANCHOR - USP HTT16 W/ (18) 10d NAILS INTO DOUBLE STUD W/ 3/8"X15" ANCHOR BOLT (TOTAL 18)

INSC 14818
 ALTHOUGH DILIGENT EFFORTS HAVE BEEN MADE IN THE PREPARATION AND CHECKING OF THESE PLANS FOR ACCURACY, ALL CONTRACTORS SHALL CHECK, VERIFY AND BE RESPONSIBLE FOR ALL DETAILS AND DIMENSIONS. ALL CONTRACTORS SHALL EXAMINE THE SITE AND EXISTING CONDITIONS OF THE PROPOSED WORK. ALL CONTRACTORS SHALL REVIEW ALL PLAN PAGES FOR WORK IN THEIR CONTRACT. ALL WORK ON THIS PROJECT IS TO BE GOVERNED BY THE LATEST EDITION OF THE CODES AND REGULATIONS. ENGINEER WILL NOT BE RESPONSIBLE FOR EXISTING OR PREEXISTING SOIL OR STRUCTURAL CONDITIONS. THE ENGINEER WILL NOT BE RESPONSIBLE FOR CONSTRUCTION METHODS, TECHNIQUES, SEQUENCES, OR PROCEDURES, OR FOR SAFETY PRECAUTIONS AND PROGRAMS IN CONNECTION WITH THE WORK, AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONTRACTOR'S FAILURE TO CARRY OUT THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. CONTRACTOR SHALL REVIEW ALL SHOP DRAWINGS PRIOR TO SUBMITTAL TO THE ENGINEER.

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 WWW.RATSCHENGINEERING.COM

PROJECT NAME:
ALTENBURG - CENTER CITY MARKET

PROJECT DESCRIPTION:
PROPOSED MERCANTILE/ASSEMBLY ADDITIONAL/ALTERATION

PROJECT ADDRESS:
2041 MADISON STREET STEVENS POINT, WISCONSIN 54481

DRAWN BY: **SL5**

CHECKED BY: **---**

PLAN SET:
PRELIMINARY

DATE:
10/27/2025

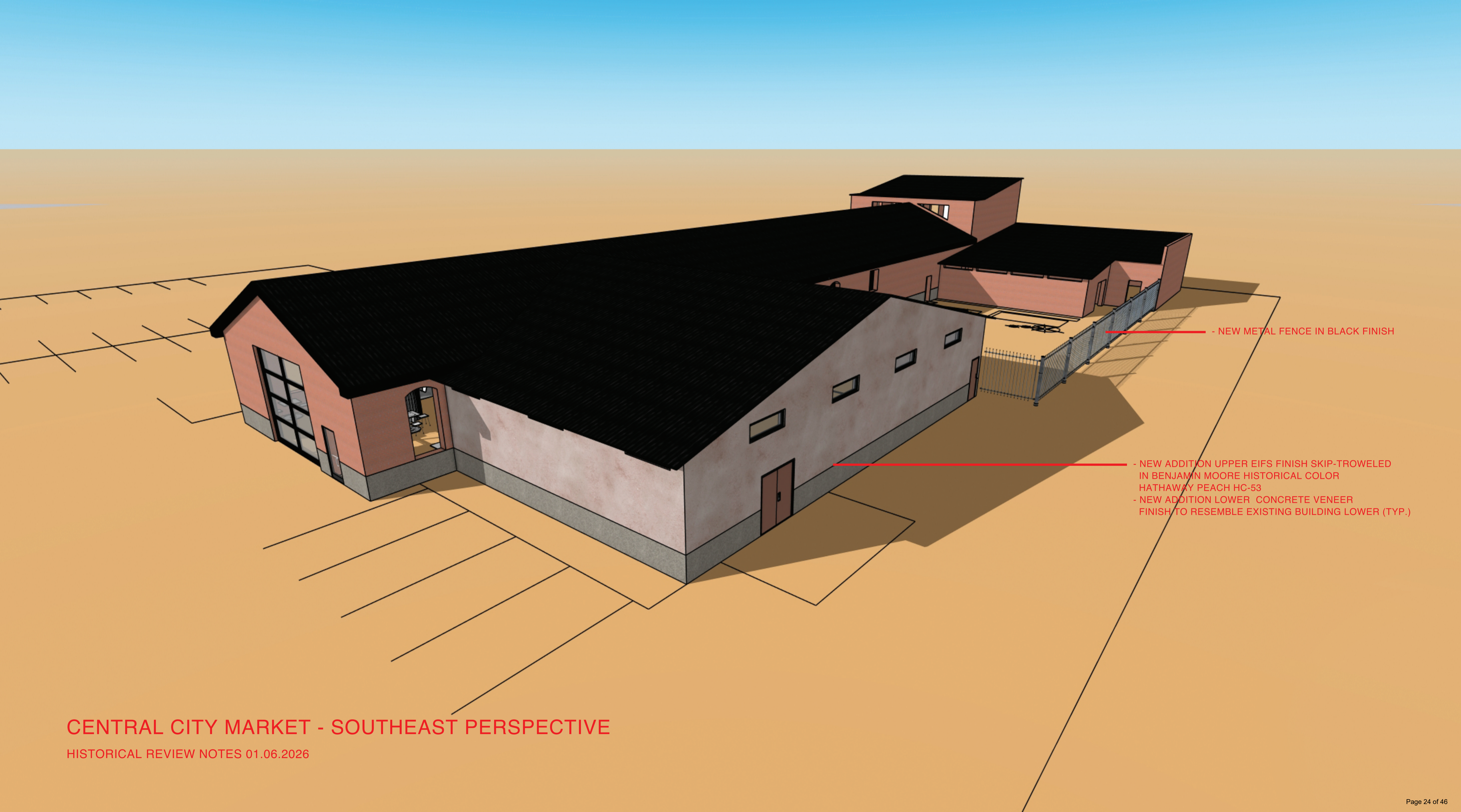
SCALE:
AS NOTED

REVISION:

SHEET DESCRIPTION:
FOUNDATION & ROOF FRAMING PLANS

SHEET SIZE: ARCH D 36X24

SHEET:
S1.1

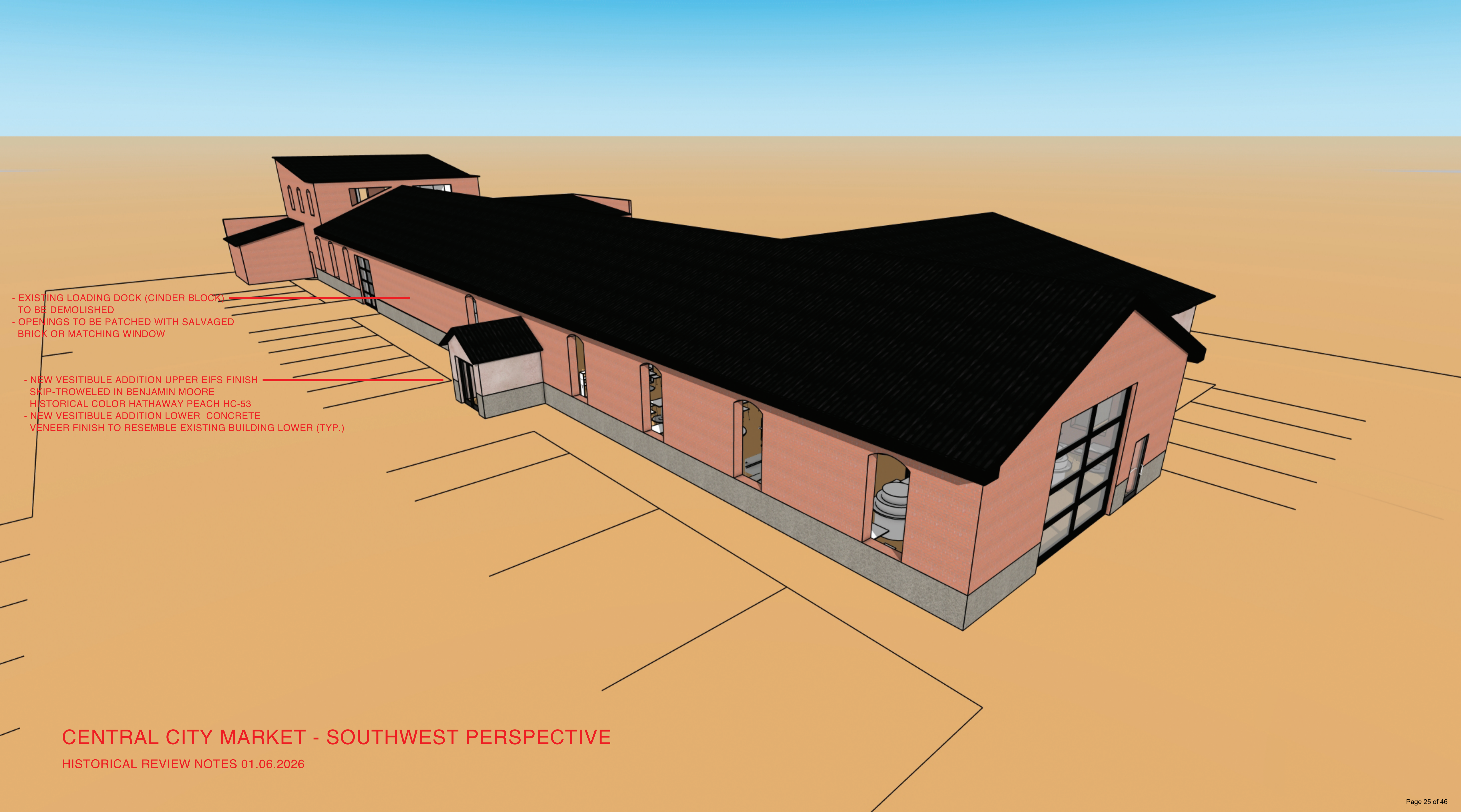


- NEW METAL FENCE IN BLACK FINISH

- NEW ADDITION UPPER EIFS FINISH SKIP-TROWELED
IN BENJAMIN MOORE HISTORICAL COLOR
HATHAWAY PEACH HC-53
- NEW ADDITION LOWER CONCRETE VENEER
FINISH TO RESEMBLE EXISTING BUILDING LOWER (TYP.)

CENTRAL CITY MARKET - SOUTHEAST PERSPECTIVE

HISTORICAL REVIEW NOTES 01.06.2026



- EXISTING LOADING DOCK (CINDER BLOCK)
TO BE DEMOLISHED
- OPENINGS TO BE PATCHED WITH SALVAGED
BRICK OR MATCHING WINDOW

- NEW VESITIBULE ADDITION UPPER EIFS FINISH
SKIP-TROWELED IN BENJAMIN MOORE
HISTORICAL COLOR HATHAWAY PEACH HC-53
- NEW VESITIBULE ADDITION LOWER CONCRETE
VENEER FINISH TO RESEMBLE EXISTING BUILDING LOWER (TYP.)

CENTRAL CITY MARKET - SOUTHWEST PERSPECTIVE

HISTORICAL REVIEW NOTES 01.06.2026

NOT LUMBER OR PAPER, BUT STILL A BIG INDUSTRY

THE PINERY IRON WORKS

As everyone knows, Stevens Point was put on the map by the logging industry, and kept there for many years by the papermaking business. But there were other enterprises as well—the railroads and Sentry Insurance come to mind. Yet another industry was a very heavy one: the making of objects of iron and steel. And Stevens Point had several, if not many, foundries or machine shops over the decades. The longest-lived of these was Richard Cook's establishment, which ran under various names for over 50 years.

When exactly the first foundry or machine shop started in Stevens Point, is hard to pinpoint, but a history of his company published in the July 15, 1893, *Stevens Point Journal*, tries. According to it, "The original plant was established in the '40s by Robb & Craig, on the banks of the Wisconsin river at the foot of Main street." Then, the article continues, that shop was sold to the William Weston & Sons sawmill, "who afterwards sold it to Merrill Whitney & Steel, who conducted it till 1874, when Saylor & Cook purchased it, and in 1879 built new shops near the Curran House, continuing business there five or six years when Mr. Saylor sold his interest to Geo. A. Packard, and after three years Mr. R.A. Cook bought Mr. Packard's interest and has since been sole proprietor. In 1890 the works burned, and Mr. Cook then purchased about 2 ½ acres of land fronting on Madison, Monroe and Reserve streets, and in the spring of 1892 erected his present commodious brick buildings...."

This history is accurate in general, but it contains some minor errors, and omits some details. No corroborating evidence has been found for Robb and Craig, the apparent founders of the city's first machine shops, nor has any information been found about them. The 1840 and 1850 U.S. censuses contain no mention of them, for example. There is lots of information about William Weston's sawmill, but none so far about his buying or owning a foundry.

The first owners mentioned in the *Journal's* sketch that can be verified in other sources are Whitney, Merrill, and Steele—that is, John Whitney, W.F. Merrill, and Garvin Steele. Their ad appeared in the *Journal* of April 20, 1875, when they called their company the Pinery Iron Works.

PINERY IRON WORKS,
STEVENS POINT, WIS.,
WHITNEY, MERRILL & STEELE, Prop's.
Manufacturers of
Steam Engines,
BOILERS,
GRIST AND SAW MILL
Machinery.
Also SHINGLE MILLS AND
Whitney's Patent
GANG EDGER,
SHAFTING, PULLEYS and GEARING
Of all kinds.
IRON AND BRASS CASTINGS.
--REPAIRS DONE PROMPTLY—
And Satisfaction Guaranteed.
We also keep in stock STEAM and WATER PIPES and FITTINGS
Brass and Iron.
Stevens Point, April 20, 1875.

In any case, they did not own or operate this business much longer. The August 14, 1875, *Journal* printed an ad announcing a "DISSOLUTION OF CO-PARTNERSHIP. The partnership existing between Whitney, Merrill & Steele is this day dissolved by mutual consent. Dated Aug. 4, 1875. JOHN WHITNEY, W.F. MERRILL, GARVIN STEELE. The business will be continued at the same place by John Whitney, who will collect and pay all the accounts of the firm. W., M., & S."

True to his statement in the dissolution notice, Whitney continued alone in business, according to the *Journal* of October 21, 1876: "MR. JOHN WHITNEY, at the Pinery Iron Works, is now [turning] out half a dozen of his celebrated shingle machines. Three of them have been ordered by Chicago parties, for use in the Michigan pineries, and some parties are contemplating sending two of them to the Black Hills."

But eventually he sold his business and apparently left town. One reason may have been the death of his wife.

Her obituary was printed in the September 9, 1876, *Journal*: “DIED. In this city, September 1st, 1876, of consumption, after a long and painful sickness extending over five years, Emily A., wife of John Whitney, aged 41 years, 9 months and 15 days.”

Only about two years later, a report in the June 2, 1877, *Journal* said that the Pinery Iron Works had “Changed Hands,” without calling the firm by its name. The report also introduced us to an owner we had not heard of before: G.W. Oaks. “The foundry and machine shops at the foot of Main street, for a few months past owned by G.W. Oaks & Co., were on Tuesday sold to Seyler & Cook. The former has been a resident of Stevens Point for the past two years or more, and has been engaged in the manufacture of boilers. The latter has been employed in the capacity of foreman at Rice Bros.’ shops. The new firm propose to start business next week.”

An ad for the Pinery Iron Works in the June 9, 1877, *Journal* confirmed the sale.

Pinery Iron Works
SEYLER & COOK, Props.
-Manufacturers of –
Steam Engines,
Boilers, Smoke Stacks and Breeching,
CIRCULAR SAW MILLS
And Mill Gearing
Of Every Description. We shall keep in stock a full line of
Steam and Gas Fittings,
ENGINE TRIMMINGS.

All work left with us will receive prompt attention, and satisfaction guaranteed. Special attention given to the
REPAIRS OF
MACHINERY AND BOILERS.
YOUR PATRONAGE SOLICITED.
Shop foot of Main street, near River.

(The July 11, 1874, *Stevens Point Journal* announced the opening of Daniel Seyler’s boiler shop, his debut in business in Stevens Point:

“The new boiler shop in this city is now open and ready for business. Mr. Seyler, the proprietor, comes to us bearing the highest recommendations from those for whom he has worked, as a first-class workman, and honest, reliable man. We trust his investment may prove a profitable one, and certain it is that the mill men in this section should give the enterprise the encouragement of their patronage....”

(Seyler’s ad, in the same issue of the *Journal*, described his services.

Daniel Seyler,
Proprietor of the
Stevens Point Boiler Works
Manufacturer of
BOILERS
Of all descriptions, sheet iron work,
IRON DOORS, SHUTTERS, ETC.
Repairing promptly done and all work
Guaranteed. Shop on Plank road, near
Depot. Stevens Point, Wis.

(Plank Road was that part of Water Street that lay south of the Wisconsin Central Railroad tracks.)

(According to the 1880 U.S. Census, Daniel Seyler was born in about 1847 in Bavaria, Germany (his wife, Ernestina, was born in Prussia), and he had five children aged nine to one years. His occupation is given as “Boiler Maker.”

(Just below Daniel’s entry in the census record is one for a Jacob Seyler, Jacob was four years older than Daniel (37 years as opposed to 33), but also born in Bavaria and also listed as a boilermaker. So the two must have been brothers, and probably worked in, or even co-owned, the same foundry.

(Daniel's son Daniel's obituary, in the December 9, 1882, *Journal*, is brief: "DIED. SEYLER—In this city Sunday morning, Nov. 25, 1882, Daniel J. son of Daniel and Ernestina Seyler, aged 5 years and 3 months.")

(One clue as to what became of Daniel Seyler, Sr., appears in an article in *The Gazette* of December 5, 1894. "The Novelty Iron Works, at Ashland, was completely destroyed by fire, last Thursday morning. The loss is placed at \$25,000, with only \$3,000 insurance. D.J. Seyler, the proprietor, removed from this city to Ashland ten years ago, and had built up a large business.")

(More evidence of Seyler's presence in Ashland is in the city directory of 1888: "Seyler Daniel J, Proprietor Novelty Iron Works Bay n e cor. Ninth ave E, res 819 St. Clair.")

(Another clue about Seyler appears in a "Special Schedule" of the 1890 U.S. Census that counted surviving Civil War veterans. According to it, Daniel Seyler was a corporal in Company M of the 7th New York Volunteer Artillery Regiment. He enlisted on January 8, 1864, and was discharged on July 2, 1865.)

(What ultimately became of Seyler—if he stayed in Ashland or moved away after his disastrous fire—is a mystery.)

But while Seyler was a partner with R.A. Cook, their firm obviously prospered. The July 10, 1880, *Journal* reported on their expansion to new facilities. "INTO NEW QUARTERS. Seyler & Cook of the Pinery Iron Works, moved into their new machine shops on the north side of Main Street the first of the week. Their shop is roomy and convenient. In the front they have an office and stock room, with an entrance on Main St.; to the left is the machine shop, at the rear of which their blacksmith shop and engine house are located. The boilers and engine are enclosed with brick walls and well protected from fire. Their foundry will be located some two hundred feet to the rear and they are now engaged in remodeling the large barn, which occupied the rear end of the lot, for this purpose. They are now prepared to execute orders with promptness, and their increased facilities and commodious quarters will aid them materially in filling orders on short notice. Give the new shop a call."



R.A. Cook & Company Foundry and Machine Shop when it was located at the west end of Main Street in Stevens Point, from 1881 to 1892. In foreground: a mower, one of the farm implements that the company stocked and sold at the time.

Dasniel Seyler sold his interest in the Pinery Iron Works to George A Packard in 1881, and the firm was renamed R.A. Cook & Company, according to the *Journal* of March 12. "BUSINESS CHANGE. The firm of Seyler & Cook, proprietors of the Pinery Iron Works, has been dissolved, Mr. Seyler having disposed of his interests to George A. Packard. The firm will hereafter be known as R.A. Cook & Co., and they will continue business at the same place. Of Dick Cook it is unnecessary to speak in [connection] with the machinists' business, as all those who have patronized the late firm during the past three years know him to be a skilled mechanic and a pleasant gentleman to do business with. George A. Packard, the new member of the firm, is known to almost everybody here and hereabouts, and while he is not a machinist by trade, he proposes to take hold and learn. And then there are many things to do which he is familiar with, such as keeping the books, attending to the correspondence, collecting, &c.,--and these are by no means a minor part to any business. The new firm already have a good prospect for plenty of work and are determined to make a success of it."

The Gazette of August 24, 1881, listed some of the kinds of products that the company made. "AT R.A. COOK & CO'S These gentlemen are now crowded with work, as indeed they have been during the past several months, and are obliged to keep their large force of men at work night and day during a good portion of the time. At present they are furnishing a boiler and engine for Messrs. Langenburg & Podach, which will be used by these gentlemen in manufacturing of brick at their yard just north of this city.... Messrs. Cook & Co. are also manufacturing new pulleys and making a number of repairs for the saw mill of W.T. Price, at Unity, making shafting, pulleys, boxes, &c., to be used in the new addition to Brown Bros. planning mill, repairing machinery for Herren & Wadleigh's planning mill, building a new bull-wheel for B.M. Holmes, of Ogema, making a new gang-edger and trimmer and doing a large amount of repairing for the saw and shingle mill of N. Boyington, in the town of Sharon, this county, as well as a goodly number of minor jobs. And still they say, let customers send on their work and they will turn it out on time, even if they have to enlarge their already commodious shops."

In 1884, R.A. Cook & Co. began selling agricultural equipment and machinery, according to an article and ad in the March 15 issue of the *Journal*. "In another column will be found the advertisement of R.A. Cook & Co., calling attention to the stock of agricultural machinery which they now have on hand. They are prepared to fill orders at once and will be pleased to have farmers call and look over their stock."

Farmers Look Here!

Are you going to need any kind of Farm Machinery?

If so, don't fail to call on

R.A. Cook & Co.,

Where you can find all kinds of

FARM MACHINERY

We are exclusive agents for the

Champion Reapers, Mowers and Binders!

Farmers when you come to the city don't fail to call at our Warehouse, below Curran House on Main street, and see sample machines in operation. We sell every kind of machine a farmer needs from a harrow to a Steam Thresher.

Don't fail to call and see us. R.A. COOK & CO.

In 1885, George Packard retired from his partnership with Richard Cook, the April 4 *Journal* reported. "As will be seen by the official announcement published elsewhere, the firm of R.A. Cook & Co. was dissolved on Thursday, by George A. Packard retiring. The business will be continued by R.A. Cook, who will give it his undivided personal attention. He is a thoroughly practical man, having been engaged in the business for eighteen years, and will do all in his power to still further extend the large business they have built up during the past four years. Mr. Packard has not fully decided what he will do. We hope he will conclude to remain here, as he is a young man of good business ability and would make a success of most any business, as he certainly has of the Pinery Iron Works.

"Joseph Pierron, who has been with the firm for several years, will be continued in the capacity of foreman. Mr. Pierron is thoroughly familiar with mill work of all kinds and a practical machinist of many years experience."

Contrary to the *Journal's* hopes, Packard did not remain in Stevens Point, but went to Hayward, Wisconsin, according to the *Stevens Point Daily Journal* of June 10, 1920. In Hayward, he "was employed in a bank and later engaged in the livery business. Moving to Bayfield, Mr. Packard became one of that city's leading merchants and served as postmaster for a period of seventeen years." Later, he bought a large farm near Knoxville, Pennsylvania.

Meanwhile, R.A. Cook took on yet another partner, C.J. Scribner of Fond du Lac, so the firm name again

became R.A. Cook & Co. The January 2, 1886, *Journal* reported that Scribner “has for the past few years been engaged as traveler for Vilas Bros., Chicago, dealers in paints and oils, and has a large acquaintance among mill men through this section of the state....”

Probably under Scribner’s influence, the firm began stocking oils for sale, according to the February 27, 1886, *Journal*. “Messrs. R.A. Cook & Co., have added a full line of oils to supply illuminating and luminating [?] oils to the trade at the very lowest market quotations. Mr. Scribner has had five years experience in handling oils and knows just what their customers want. They want their goods first-class in every respect.”

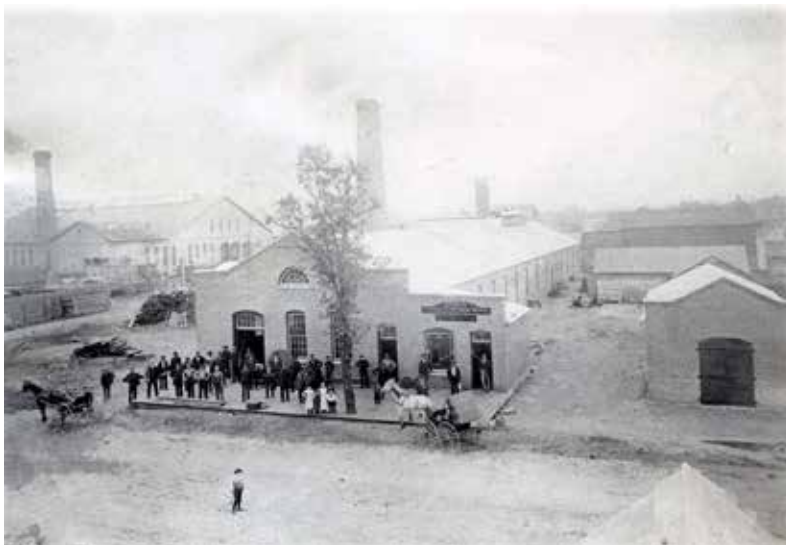
Likewise, the company began stocking quantities of paint, the June 26, 1886, *Journal* reported. “PAINT! PAINT! By a special arrangement with the manufacturers we have purchased a large quantity of mixed paints which we are offering at an unusual low price of \$1.10 per gallon in quantities to suit. This paint is made of pure lead and linseed oil, with sufficient coloring-ground in to give the desirable shade. This is the lowest price for which this quality of paint can be bought by jobbers and if you want to paint, now is your time to buy. Remember this paint is warranted not to [peel], crack or blister and will give satisfaction every time. We propose to lead in low prices and will never be undersold by anyone. R.A. COOK & CO.

Despite this stocking of smaller hardware retails items, other news reports made it clear that the firm was still very much a heavy industry. For example, the *Journal* of April 24, 1886, reported that “R.A. Cook & Co. have taken the contract for building a complete saw mill for H.L. Beese of Butternut. They have already commenced work on some of the machinery and this job together with other mill work now in their shop will keep all hands going for several weeks.”

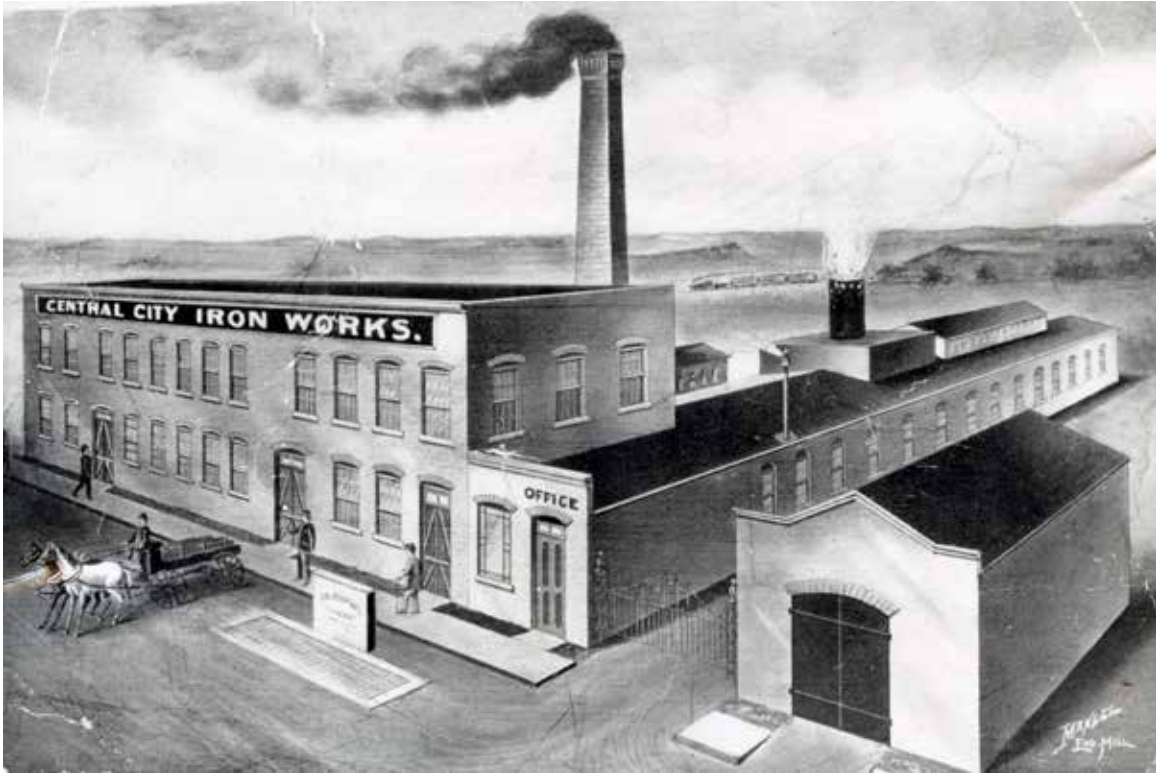
But at the end of the year, Cook’s partnership with Scribner also dissolved. According to the December 11, 1886, *Journal*, “The firm of R.A. Cook & Co., was dissolved on Monday last by C.J. Scribner retiring. The business will be continued by R.A. Cook, who has been at the head of the establishment for the past nine years. He is a first-class workman himself, and has always in the past and will in the future give the business his undivided attention. He has excellent facilities for turning out saw mill work of all kinds, and will be pleased to receive a call from those needing anything in his line.”

In 1890, Cook’s foundry was largely destroyed by fire, according to *The Gazette* of October 1. “At about three o’clock last Thursday morning the machine shop of R.A. Cook was discovered on fire, the alarm being first sounded by the blowing of the Week Lumber Co. saw mill whistle. Soon the whole upper part of the structure was in flames, but this was subdued by hard work upon the arrival of the fire department. Upon the second floor hundreds of valuable patterns were stored.... These were very dry, and proved most acceptable fuel for the flames. The building, which was a wooden structure, was saved in a wrecked and entirely [worthless] condition, and will have to be replaced with brick or stone. The machinery was also considerably damaged, including the engine and boiler, in the rear.... The property was insured for \$8,000, of which \$4,500 was in Hyde’s agency, and \$3,500 in Buckingham’s agency. The adjusters are now in the city for the purpose of fixing matters up with Mr. Cook, but as yet they have not agreed upon terms. Until then he cannot determine what will be done about rebuilding, but a new structure will undoubtedly take the place of the present ruins.”

In fact, Cook was to rebuild but not on his old site. He would pick a new site and a new name for his firm.



Workmen pose for a photo of the Central City Iron Works as it looked originally, on Madison Street on Stevens Point’s South Side. Photo taken sometime after 1892.



The enlarged, remodeled Central City Iron Works building as depicted in an idealized postcard picture made sometime before 1919.

R.A. COOK'S CENTRAL CITY IRON WORKS

After the fire in his Main Street machine shops in September of 1890, it was almost two years before Richard Cook was ready for business in his new foundry, at his new location on Madison Street, on Stevens Point's South Side. But when he finally started his new facility up, it was to be larger, more modern, and with far more business than he had at his old one.

One reason for his increased volume of business was that he had secured a contract to make large castings for the Wisconsin Central Railroad, whose extensive yard bordered his own new grounds. Indeed, that proximity was one reason he built his new foundry where he did.

All this information is contained in a long, detailed article in the February 20, 1892, *Stevens Point Journal*. According to it, Cook's contract with the railroad meant an addition of \$60,000 to \$70,000 a year to his budget, a very large sum for the time.

For his new plant, he "purchased the six lots between Madison and Monroe streets, adjoining the Central shops [where railroad cars were made and repaired].... The new shop will be 40 x 210 feet. Eighty feet on the south end will be divided off for a foundry. The balance of the building will be used as a machine shop, wood-working shop and blacksmith shop. The engine and boiler room, which will be 14 x 24 feet, will be located on the east side. Both buildings will be solid brick with iron roof. The foundry will be supplied with a Collau furnace, one of the best made, with a capacity of from eight to ten ton daily. Both the foundry and machine shop will be fitted up with the very best machinery and appliances for doing first-class work. Mr. Cook does not expect to confine himself to railroad work, but will do a general custom business as he did before he was burned out.... In fact he expects to do more than double the amount of work he formerly did, in addition to the railroad work. His facilities in all departments will be greatly increased and the fact that he will cast every day, will bring a large amount of foundry work here which now goes below [south], and this will naturally tend to increase the work in the machine shop. Work on the new shop will be commenced as soon as the frost goes out of the ground. When fully equipped, the establishment will employ from thirty to forty men."

The following issue of the *Journal*, February 27, reported that "R.A. Cook's new foundry and machine shops has been christened the Central City Iron Works."

Six months later, the whole manufacturing complex was finished, the August 13, 1892, *Journal* announced, devoting an entire column to details about it. "The main building, which rests on a stone foundation, is of red brick and

is 212 feet long and 40 feet wide, the walls being 14 feet high. The roof is of the Howe truss pattern, the timbers being very heavy. The roof is covered with asbestos roofing, as well as all the smaller buildings. The office, which is located on the west end of Madison street, is 16 feet square and is neatly fitted up. The main building is divided into a foundry, blacksmith shop and machine shop.... The boiler house on the east side is 20 x 30 feet and contains a brand new 80-horse power tubular steel boiler, made by the Pennsylvania Boiler Works, Erie, Pa. The smokestack is 9 feet square at the base, has a 36-inch flue and extends skyward 75 feet. East of the building is a shed 24 x 80 feet, partitioned off for coal, iron, &c.... The pattern storehouse, a brick fire-proof building 24 x 40 feet, is located just west of the office.... Every pattern will be numbered and any casting can be duplicated at any time. The foundry is supplied with a new Calliau cupola, with a guaranteed capacity of seven ton per hour for four consecutive hours. The cupola is 40 feet high and 60 inches in diameter, near which is an elevator with a capacity of seven tons, to be used in hoisting the iron and coal to the platform. The blast for the furnace is furnished by a No. 9 steel pressure blower.... The wood working shop will be supplied with lathes, band saws, &c., for doing all kinds of wood turning and other wood work.... A track will run through the entire length of the foundry, on which the melted iron can be carried and deposited on either side. This track will also extend into the coal and pig iron warehouse.... The shop presents a handsome appearance from the outside and being supplied with 33 large windows is well lighted and makes a very pleasant place to work. The brick work on the building was done by Ule Bros., the carpenter work by Chas. Packard and the painting by Krembs Bros. The brick was furnished by the Stevens Point Brick Co.... At present 22 hands are employed, but it will require from thirty to forty hands to do the work when all departments are under full headway...."

In the following summer, the July 15, 1893, *Journal*, in yet another long article on the Central City Iron Works, listed the products the firm had made for various companies around the state, since it had opened. "Among the work recently turned out by this establishment was a lot of shafting, gearing and pulleys for the Ellingson mill of Stetsonville, Wis.; a new granulator and a clay dumping car for Wm. Langenburg & Son's brick yard....; repairing of an engine for the Chilli Lumber Co., of Chilli, Wis.; the truss rods and roof irons and the cast iron work for the Plover Paper Co.'s mill; the shafting, pulleys, gearing boxes, etc., for Gilkey Anson & Co.'s saw mill at Merrill; repairs for locomotive, new brasses, etc. for J.J. Kennedy of Rib Lake; a 40 horse power boiler, fronts, grates, stacks and fixtures, for Whitney & Wells' mill on the Plover river; repairing of Bosworth & Reilly's gang edgers, and new braces for the engine, doors and plates for the charcoal kilns of the Wisconsin Furnace Co. on the line of the Central railroad. Also manufactured several set[s] of of the Wendt patent automatic car couplers, of which there is 10 refrigerator cars on the Wisconsin Central line equipped. A pair of these couplers are mounted and on exhibition in the transportation building at the World's fair. These works also furnish the castings for the Marshfield Iron Works, and are filling a large order for the new paper and pulp plant at Nekoosa, Wis., which consists of about 115 tons of wrought iron truss rods, bolts, plates, anchors, cast iron columns, lintels, caps, plates and washers. Ten years ago, Mr. Cook built the North Side Lumber Co.'s 75 horse power engine, and it is still in good running order...."

Cook's company operated through the 1890s and the first decade of the 20th century, increasing its business steadily. At one point, in 1909, a consortium of Stevens Point businessmen leased the Central City Iron Works plant, according to the *Gazette* of May 26, but apparently the firm continued to be called by its old name. At least the 1908, 1912, 1915-16, and 1920 city directories listed the company by the name of Central City Iron Works, and gave R.A. Cook as its proprietor.

In 1913, the company entered into a contract with the Soo Line Railroad, to furnish all its gray iron castings, a change constituting a big boost in the firm's volume of business, the March 8 *Journal* reported.

Interestingly, the Central City Iron Works indirectly participated in World War I. According to the April 7, 1917, *Journal*, "More war munitions manufacture is to be carried on in Stevens Point this season, at least indirectly. The Central City Iron Works has received a contract for building two large Wagner looms with trimmers for the Burlington Blanket Co. of Burlington, Wis. These are 57 and 80 inch machines and are designed to turn out blankets, horse collar pads and other articles. They are intended for use in the Canadian factory of the company and it is understood they will be used in making blankets and other supplies for the Canadian and British armies in Europe...."



The remains of the Trenam Tractor Company building at 2041 Madison Street in Stevens Point now. The former Altenburg Ice Cream Shop occupied the small part at the far right—once upon a time R.A. Cook's Central City Iron Works office--in the 1990s. Note that the remains of the company's name are still visible at the top of the building: "...ctor Co.

THE TRENAM TRACTOR COMPANY

After America entered World War I, the Central City Iron Works was making a more direct contribution to the war effort, but that was after it was acquired by the Trenam Tractor Company. *The Gazette* of December 26, 1917, reported that "Stevens Point is to be the home of the Trenam Tractor Co. This announcement, which had been expected for several weeks, was made [on December 20], following the closing of a deal for the purchase by the company of the land, buildings and equipment of the Central City Iron Works on Madison street. Possession was taken Friday morning [December 21].

"Although the plant was purchased ...for the manufacture of Trenam tractors...the general foundry business will be continued and enlarged. Ralph Cook [son of R.A.] is acting superintendent of the plant and F.M. Schuler of Milwaukee acting manager, the latter being secretary and treasurer of the company. The crew of twelve or fifteen is retained.

"The Trenam Tractor Co. comes to Stevens Point without a bonus of any kind, but a sufficient amount of stock was disposed of locally to purchase the plant. R.A. Cook, proprietor of the Central City Iron Works, has a financial interest in the company. G.A. Gullickson of this city, who became interested in the tractor at the time it was in the blue print stage and who is now one of the directors, was largely responsible for bringing the concern here....

"The patterns of the company are all complete and will be shipped here from Ludington, Mich., and Milwaukee within the next week. Some changes must be made in the plant and new equipment installed, but it is hoped to begin casting for the tractors within the next couple of weeks. An inventory is now being taken at the plant.

"The officers of the company do not intend to rush things, but to start in a conservative way to ensure a healthy growth. Their goal is to have the first machine of local manufacture ready by April 1 and to be able thereafter to turn out the finished tractors at the rate of five a day. The machine will be handled through dealers and the fact that she sales will

be on a cash basis will enable them to get quick returns.

“By the middle of the summer, it is hoped, from forty to fifty men will be employed. It is believed also that it will be necessary to erect another building during the coming season. If the business develops as expected, it is not improbable that 300 men will be employed inside of two years.

“The company now has five machines on hand, which have been put to the hardest kind of tests under actual field conditions, with complete success. The Trenam is a farm tractor of many uses and [develops] 12-24 horse power. It is an ‘all-purpose’ tractor, serving as a power unit as well as a substitute for horse power. It is light in weight, well balanced and mechanically a wonder, according to recognized authorities. The fact that there is an ever growing field for tractors, makes the manufacture of the Trenam a most attractive proposition....

“All of the directors are heavily interested financially. The concern is incorporated under the laws of South Dakota and has an authorized capital of \$400,000, of which \$300,000 is common stock and \$100,000 preferred.”

True to its word, the tractor company did continue to do general foundry and machine-shop work. So its ad announced in the January 16, 1918, *Gazette*:

Foundry and Machine Work
Having taken over the Central City Iron Works,
we have the facilities for giving prompt and first-
class service in all kinds of Foundry and Machine
work.
We solicit your patronage and guarantee satisfaction.
TRENAM TRACTOR COMPANY
SOUTH SIDE

Also, according to the May 15, 1918, *Gazette*, the plant began a more direct involvement in war work. “STEVENS POINT FIRM GETS WAR CONTRACT.... Grey iron castings, engine parts for coast defense boats, are now being manufactured in Stevens Point by the Trenam Tractor Co., which took over the plant of the Central City Iron Works last fall.

“The local concern is turning out the castings for the Allis Chalmers Co. of West Allis, which has for many months been utilizing its immense plant for war work. The volume of war work being sent here is increasing and it is expected that within a short time casting will be done daily. Twenty men are now employed....”

But, otherwise, the tractor company signally failed to live up to its promised success. Contrary to its glowing predictions, it went bankrupt only a year later, and made only one demonstrator tractor, according to the October 12, 1966, *Daily Journal*. “In 1919, the company lost its license to do business in Wisconsin for failing to file an annual report.” An intensive search of the local newspapers from 1919 through 1921 has failed to corroborate these statements.

Other evidence has surfaced that the company did not meet its stated expectations. For example, an article in the May 7, 1920, *Journal*, reporting on a meeting of the Trenam Tractor Company’s board of directors, conceded that the firm was making no tractors at that time. “While the Trenam Tractor plant in this city is not now engaged in the manufacture of tractors, as changes and improvements in the machine room are being made by the draftsmen, both the local foundry and machine shop are being run full time....”

Further evidence—albeit circumstantial—is an ad in the *Stevens Point Gazette* of November 9, 1920: “FOR SALE To the highest bidder 100 Shares Trenam Tractor Stock. Write J.S. CUDLIPP 1834 Main St. BUFFALO, N.Y.” This ad does not prove the company was going bankrupt, but it does appear that at least one stockholder was trying to jump from the ship before it sank.

And was it only a coincidence that at virtually the same time as the Trenam Tractor Company was failing, a new tractor factory sprang up in Stevens Point? That is what readers of the *Gazette* of November 9 and 16, 1920, were told. An ad in the issue of November 9 announced a “TRACTOR DEMONSTRATION at the R.K. McDONALD FARM west side THURSDAY, NOV. 11 2 P.M. By the FARMERS’ TRACTOR CORP. New Industry Located Here.”

In a full-page ad in the November 16 *Gazette*, the new company announced a capitalization of \$500,000, with preferred stock offered for sale at \$100.00 per share, “with a bonus of one share of the common stock.... The citizens of Stevens Point and vicinity can easily ...hasten the day of early manufacture and returns. INVEST TODAY.” But the ad was vague about much important information, including where exactly in the city the tractors would be made.

After the Trenam Tractor Company went bankrupt and into receivership, two new firms set up business successively in the building. The first was a carton- or box-making factory, the Wisconsin Carton Company, which was announced in the March 22, 1929, *Daily Journal*. This company secured a three-year lease on the former tractor-factory building, and, the May 8 issue reported, laid a new concrete floor in part of the facility.

Ten years later, “The Vaudell Wholesale Food company, a new concern, has located in Stevens Point and is occupying quarters in part of the Trenam Tractor company building at 319 Madison street,” the June 20, 1939, *Daily Journal* reported. But neither of these companies apparently lasted very long.

Meanwhile, the progressive decline of Trenam Tractor Company building came in two or three fires that damaged the structure that was the company’s only remaining asset. The last fire was in 1959, the *Daily Journal* of February 12 reported. Then the building was used mostly as a warehouse, and was rented by various local companies for storage. But it burned extensively, and part of the second floor collapsed onto the first floor.

Not until 1968 did the stockholders from 1918, or their descendants, get back the money they had invested in Trenam Tractor, and then only pennies on the dollar, reported the February 15, 1968, *Daily Journal*. It was an anticlimax from the optimistic buildup 50 years earlier, especially for one investor, who was killed in a car accident in Florida just as she was to receive her settlement check.

The Trenam Tractor Company building—or part of it—still stands on the south side of Madison Street, with some of the letters of its name still visible on its red-brick façade. That is, also, part of R.A. Cook’s Central City Iron Works building still stands, for they are or were the same building. And what was Richard Cook’s office, in the small west section of the building, was in the 1990s the Altenburg Dairy Ice Cream Shop. All is deserted and quiet now, belying the busy hum and clank and thud of heavy machinery of a century ago, when massive machines made parts for train engines, sawmills, bridges, and buildings five stories high.



Richard A. Cook (1850-1926) was born in England. He was part-owner of the Pinery Iron Works and the R.A. Cook & Company firm, and sole owner of the Central City Iron Works.



Delia Damp Cook (1854-1938) was Richard .A. Cook’s second wife.

THE LIFE OF RICHARD COOK

In its general outlines—because we know little of its details—the life of Richard A. Cook of Stevens Point would seem to have been the quintessential rags-to-riches success story of American folklore. He began life humbly, and ended it in at least local prominence and wealth.

According to his obituary in the March 5, 1926, *Stevens Point Daily Journal*, Cook was born in Netherton, Yorkshire, England, in 1850. When he was five years old, he emigrated with his parents and young sister to America, “the ocean trip being made in a sailing vessel which required 13 weeks and three days to cover the distance between Liverpool and New York.” Unfortunately, his sister died during the voyage.

Cook’s obituary says his family settled in Burlington, Wisconsin, when they arrived in America, and remained there twelve years. Then they moved to Fond du Lac, where young Richard began learning the machinist’s trade “in the Trowbridge shop.” Afterwards, he worked for the Northwestern Railroad Company as a machinist.

According to his obituary, Cook had, besides his business in Stevens Point, investments in “a veneer plant at Glidden [Wisconsin] and also was part owner of extensive timber properties in northern Minnesota.”

Cook was also active in the social and civic life of the city. He was a 32nd Degree Mason, and was the “treasurer of the local bodies.” “Mr. Cook was also prominently identified with the congregation of the Church of the Intercession, serving successively as vestryman, junior and senior warden.... In civic affairs the deceased did his full share. He served as alderman for a number of years and also represented the Second Ward on the local board of education, in which latter body he filled the office of president for a longer period than any other local resident.”

Despite his material success, Richard Cook, like most people, experienced his share of life’s pain and losses. Besides his sister’s early death, his father died while he, the son, was still living in Fond du Lac, according to his mother’s obituary in the *Stevens Point Journal* of October 30, 1895. (His parents apparently had two more children after they came to the United States, one of whom also was a resident of Stevens Point: Etta—Mrs. John—Shaffer.)

Also, Richard Cook’s only child, a boy, died at the age of two months, the June 14, 1884, *Journal* reported. And four years later, Cook’s first wife, Eliza, died of consumption—tuberculosis—at the age of 37, according to the October 6, 1888, *Journal*. Her maiden name was Trowbridge; was she related to her husband’s first employer? She was a teacher before she married R.A. Cook, her obituary noted, and they were married in 1882, the same year their fine, red-brick Italianate house at 1532 Church Street was built.

In 1890, Richard Cook remarried. His new bride was Delia Damp, also a teacher before she married Cook. She grew up on farms near Juneau, Pardeeville, and Lowell, and eventually moved to Portage, Wisconsin, with her parents, where she graduated from high school. She also studied teaching for a year at the Oshkosh Normal School in 1875, before moving to Stevens Point to teach, according to her obituary in the April 6, 1939, *Daily Journal*. However, after teaching here for a few years, she left to teach at Oshkosh and Medford for three years each before returning to Stevens Point permanently and marrying R.A. Cook.

Delia Cook was active in many social organizations, her obituary says, including playing-card groups, church groups, and Progress Club, a literary society. Also, “Instrumental in the establishment of a public library in Stevens Point, Mrs. Cook was elected to the board in 1898 and was chosen as its secretary at the first meeting.... She was... president of the board for many years.”

The late Marjorie Warner, director of the first Charles M. White Memorial Public Library, recalled that when Delia Cook was president of the Stevens Point Library Board, she, Mrs. Cook, would wear white gloves into the then-Carnegie Public Library. If she saw dust on the windows, she would write with her white-gloved finger in the dust, “Delia D. Cook, Delia D. Cook” in big script, so that everyone would notice it. And the next time she came into the library, the windows were clean.

R.A. and the second Mrs. Cook had one child, a son, Ralph. According to his obituary in the *Journal* of May 21, 1983, Ralph graduated from high school in 1912, and attended the Stevens Point Normal School. He served in the U.S. Army during World War I, and during World War II “was a government inspector in defense plants.” He worked for his father’s Central City Iron Works and the Trenam Tractor Company, as well as the Consolidated Paper Company. He also sold Buick cars and GM trucks in Stevens Point, and was City Assessor from 1953-1960, his obituary says.



HISTORIC PRESERVATION/ DESIGN REVIEW COMMISSION

City of Stevens Point
Community Development Department

1515 Strongs Avenue, Stevens Point, WI 54481
P: (715) 346-1567 F: (715) 346-1498
communitydevelopment@stevenspoint.com
<http://stevenspoint.com>

APPLICATION FOR DEMOLITION

ADMINISTRATIVE SUMMARY (Staff Use Only)

Application #		Date Submitted		Assigned Case Manager	
Associated Applications (if any)				Pre-Application Conference Date	
Decision		Date Reviewed		Staff Signature	
Notes:					

APPLICANT/CONTACT INFORMATION

APPLICANT INFORMATION		CONTACT INFORMATION (Same as Applicant? <input type="checkbox"/>)	
Applicant Name	City of Stevens Point	Contact Name	Jarod Kivela
Address	1515 Strongs Ave	Address	1515 Strongs Ave
City, State, Zip	Stevens Point, WI 54481	City, State, Zip	Stevens Point, WI 54481
Telephone	715-346-1570	Telephone	715-346-1567
Fax		Fax	
Email		Email	jkivela@stevenspoint.com

OWNERSHIP INFORMATION

PROPERTY OWNER OF RECORD 1 INFORMATION (Same as Applicant? <input type="checkbox"/>)		PROPERTY OWNER OF RECORD 2 INFORMATION (If Needed)	
Owner's Name	Portage County (to be transferred to City on 1/20/2026)	Owner's Name	Property to be transferred to City of Stevens Point on 1/20/26
Address	1516 Church Street	Address	1515 Strongs Ave
City, State, Zip	Stevens Point, WI 54481	City, State, Zip	Stevens Point, WI 54481
Telephone		Telephone	715-346-1567
Fax		Fax	
Email		Email	

PROJECT SUMMARY

Subject Property Location [Please Include Address and Assessor's Identification Number(s)]		
Parcel 1	Parcel 2	Parcel 3
281240832202053		
Legal Description of Subject Property		
LOT 1 CSM#4437-15- 232 INC OL 2 CSM#6565-24-88 EX OL1 & OL3 CSM 24/88 & EX PRCL 37'X139.34'IN OL10 OF STRONG ELLIS & OTHERS -695/154 ACC RTS IN OL2&3 CSM24/88 796321		
Area of Subject Property (Acres/Sq Ft)		Area of Building or Structure (Sq Ft)
Acreage: 1.24		Approx. 57,000 sqft building (3 story)

Current Zoning District(s)		Current Historic District(s) - Local, State, National	
Institutional		Downtown Design Review District	
Designated Future Land Use Category	Current Use of Property		Proposed Use of Property
Downtown District	Vacant/Storage for Portage County		City of Stevens Point City Hall Offices
Briefly describe the property and building(s) Please also provide reasoning for the demolition request, along with the time schedule (if any) for demolition. (Use additional pages if necessary)			
<p>The property at 1039 Ellis Street contains an older commercial/industrial-era building that has been substantially modified over time for private office use. While functional for limited purposes, the building's age, layout, structural systems, and prior alterations make it poorly suited for modern municipal operations. The existing configuration does not efficiently support contemporary City Hall needs, including public accessibility, secure public-facing spaces, interdepartmental workflows, technology infrastructure, and long-term operational efficiency. Demolition is requested because adaptive reuse of the existing structure would require extensive and cost-prohibitive renovations to meet current building codes, accessibility standards, security requirements, and space programming necessary for a fully functioning City Hall. Retaining the structure would also limit the City's ability to design a facility that meets operational needs, supports efficient service delivery, and accommodates future growth. Removal of the existing building will allow the site to be redeveloped in a manner that is purpose-built, efficient, and aligned with long-term municipal needs. It should also be noted that renovations over the years have significantly reduced any historically significant building elements or architecture that the building once had. Demolition is anticipated to start in January 2026, following receipt of all necessary approvals and completion of contractor procurement. Once initiated, demolition and site stabilization activities are expected to be completed by mid May.</p>			
Please describe the building or structures architectural or historical significance (if any).			
<p>Constructed in 1928, the building at 1039 Ellis Street reflects Stevens Point's early 20th-century industrial development and its close ties to the local fishing-tackle manufacturing industry. The structure was erected during a period when Ellis Street served as a hub for light industrial and manufacturing uses, most notably those associated with tackle production that helped define Stevens Point as the "Fly Tackle Capital of the World." The site is historically linked to the nearby operations of the C.J. Frost Fishing Tackle Company, whose business was acquired by the Weber Tackle Company in the mid-1920s. In 1928, Weber expanded its operations to facilities along Ellis Street, coinciding with the construction of the building at 1039 Ellis Street. While the building has since been remodeled and its original industrial character altered, it remains representative of the era when manufacturing and tackle production played a significant role in Stevens Point's economic identity and physical development.</p>			
Does the building or structure contribute to the architectural or historic character of the historic district as a whole?			
<p>No. In our opinion, while the building is of an age that overlaps with the historic period of the surrounding area, it has been significantly modified over time, and character-defining historic features have been removed or obscured through past renovations. As a result, the structure no longer retains sufficient architectural integrity to convey its original design, materials, workmanship, or historic industrial character. Consequently, the building does not contribute to the architectural or historic character of the historic district as a whole. The new building design is set to incorporate elements from the original structure when it was Weber Tackle.</p>			
Does the building or structure display old, unusual or uncommon design, texture, and/or materials?			
<p>No. In our opinion, the building does not display old, unusual, or uncommon design features, textures, or materials. Due to significant alterations and renovations over time, original architectural elements and materials have been removed, covered, or replaced with standard modern finishes. As a result, the structure does not retain distinctive design characteristics or material qualities that would be considered architecturally unique or uncommon.</p>			
Does the building or structure promote or encourage the study of American history, architecture and design or assist in developing an understanding of American culture and heritage?			
<p>No. The building does not promote or encourage the study of American history, architecture, or design, nor does it meaningfully assist in developing an understanding of American culture or heritage. Although the structure dates to an earlier period, extensive alterations have removed or obscured the features necessary to interpret its original form, function, or historical context. As a result, the building no longer conveys sufficient historic or architectural integrity to serve as an educational or interpretive resource.</p>			
Describe the building or structures current condition and provide rationale and cause (if any) for its current state. Photographs are encouraged.			
<p>The building is in fair to poor condition and reflects the cumulative impacts of age, deferred reinvestment, and multiple prior renovations. While portions of the structure remain serviceable, the building exhibits outdated mechanical, electrical, and plumbing systems, inefficient and fragmented interior layouts, and materials nearing the end of their useful life. Past alterations have removed or obscured original architectural features, resulting in a structure that lacks cohesion and functional efficiency. The building does not meet modern expectations for accessibility, security, energy efficiency, or technology infrastructure, and continued use would require substantial capital investment. The primary causes of the building's current condition include its age, incremental modifications made to accommodate changing uses, and the absence of comprehensive modernization over time. These factors have contributed to operational inefficiencies and increasing maintenance challenges, making rehabilitation impractical relative to replacement for contemporary municipal needs.</p>			
What is the anticipated land use or proposed development (if any) for the property?			
<p>The anticipated land use for the property is municipal and civic use, specifically the development of a new City Hall facility. Following demolition of the existing structure, the site is intended to be redeveloped with a purpose-built City Hall designed to meet current and future operational needs of the City. While the existing building does not retain historic integrity, the proposed development intends to incorporate historic based materials and architectural design elements as part of the new construction. These features will serve as a tribute to the site's industrial past and Stevens Point's historic tackle-based manufacturing businesses, allowing the City to acknowledge and interpret the area's heritage while delivering a modern, efficient civic facility. Review of this proposed structure will be provided to the Historic Preservation Commission during a February meeting. City Staff intends to meet with the Historic Chair and send out materials to the body prior to the meeting.</p>			

Please describe any non-conformities that exist on the property or structure (if any), regarding building and zoning regulations, as well as architectural design guidelines.

The existing building contains several non-conformities related to current building codes, accessibility standards, and site design expectations, largely due to its age and incremental alterations over time. These include deficiencies in accessibility, outdated building systems, and interior configurations that do not align with contemporary life-safety, security, and operational requirements. In addition, the site layout and building orientation do not fully conform to current zoning standards or architectural design guidelines intended to support modern civic or commercial development. While these non-conformities may have been legally established under prior regulations, addressing them through rehabilitation would require extensive modifications. Redevelopment of the site will allow future improvements to be designed in full compliance with applicable building codes, zoning regulations, and architectural design standards.

EXHIBITS

Letter to District Alderperson (www.stevenspoint.com/directory)	<input type="checkbox"/>	Additional Exhibits If Any (List):
Photographs of Building or Structure	<input checked="" type="checkbox"/>	
Site Plan (for any future improvements)	<input checked="" type="checkbox"/>	
Historic Photos, Renderings or Maps	<input type="checkbox"/>	

CERTIFICATION AND SIGNATURE

By my signature below, I certify that the information contained in this application is true and correct to the best of my knowledge at the time of the application. I acknowledge that I understand and have complied with all of the submittal requirements and procedures and that this application is a complete application submittal. I further understand that an incomplete application submittal may cause my application to be deferred to the next posted deadline date.

Signature of Applicant	Date	Signature of Property Owner(s)	Date
	1/9/26		1/9/26



Ellis St
Streams Way





Ⓟ

IDEA
SCIENCE







PROPERTY RECORD

1039 ELLIS ST

Architecture and History Inventory



NAMES

Historic Name:

Other Name: **Weber Manufacturing Co.**

Contributing:

Reference Number: **73271**

PROPERTY LOCATION

Location (Address): **1039 ELLIS ST**

County: **Portage**

City: **Stevens Point**

Township/Village:

Unincorporated Community:

Town:

Range:

Direction:

Section:

Quarter Section:

Quarter/Quarter Section:

PROPERTY FEATURES

Year Built: **1928**

Additions:

Survey Date: **2011**

Historic Use: **large office building**

Architectural Style: **Commercial Vernacular**

Structural System:

Wall Material: **Stucco**

Architect:

Other Buildings on Site:

Other Buildings On Site:

Demolished?: **No**

Demolished Date:

NATIONAL AND STATE REGISTER OF HISTORIC PLACES

National/State Register Listing Name: **Not listed**

National Register Listing Date:

State Register Listing Date:

NOTES

Additional Information: THIS WAS ORIGINALLY A BRICK-CLAD INDUSTRIAL BUILDING THAT THE ASSESSOR'S RECORDS SAY WAS BUILT IN 1928. SINCE THEN, HOWEVER, IT HAS BEEN COMPLETELY REMODELLED INTO OFFICE SPACE, THE EXTERIOR HAS A CONTEMPORARY STYLE DESIGN AND IS CLAD IN STUCCO PANELS, AND THE BUILDING NO LONGER RETAINS ITS HISTORIC APPEARANCE.

Bibliographic References: CITY OF STEVENS POINT ASSESSOR'S RECORDS.

RECORD LOCATION

Wisconsin Architecture and History Inventory, State Historic Preservation Office, Wisconsin Historical Society, Madison, Wisconsin

Have Questions?

If you didn't find the record you were looking for, or have other questions about historic preservation, please email us and we can help:

leah.penzkover@wisconsinhistory.org

If you have an update, correction, or addition to a record, please include this in your message:

- AHI number
- Information to be added or changed
- Source information

Note: When providing a historical fact, such as the story of a historic event or the name of an architect, be sure to list your sources. We will only create or update a property record if we can verify a submission is factual and accurate.